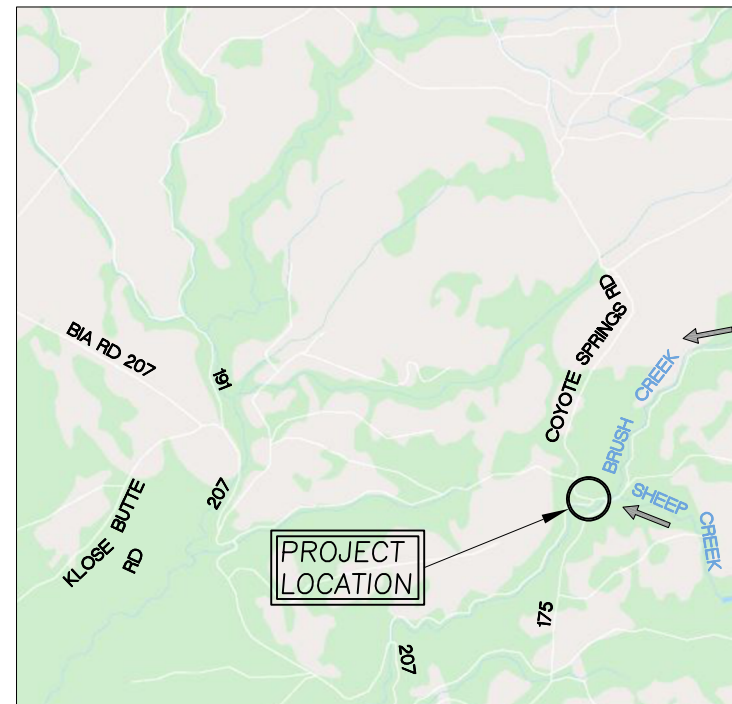
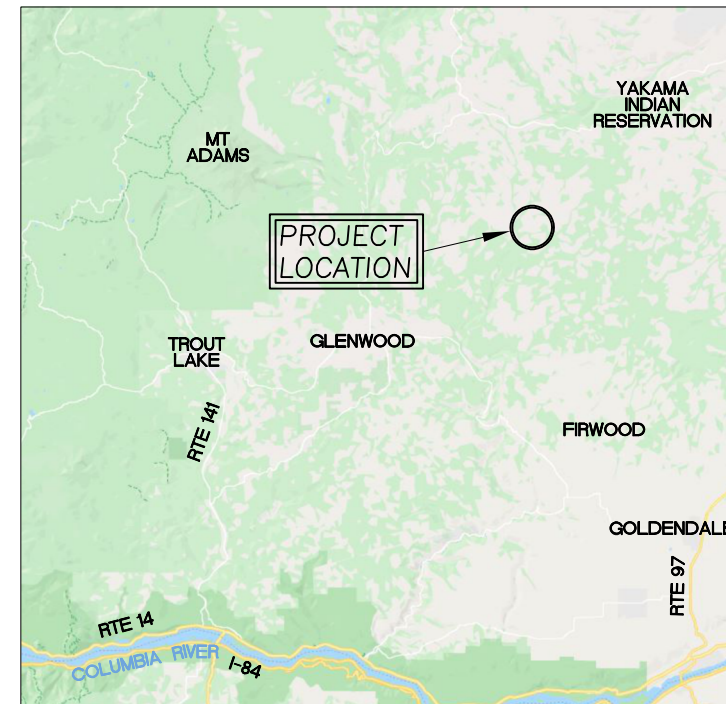


BRUSH CREEK FISH PASSAGE AT 175 ROAD PROJECT

100% DESIGN SUBMITTAL REVISION 2



VICINITY MAP
N.T.S. (GOOGLE)



REGIONAL MAP
N.T.S. (GOOGLE)

SHEET INDEX

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- C2 EXISTING CONDITIONS
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- C4 CREEK SECTIONS
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- C6 ROAD PROFILE AND ELEVATIONS
- C7 EROSION CONTROL AND DEWATERING PLAN
- C8 DETAILS AND NOTES
- C9 NOTES
- C10 HIP 4 GENERAL CONSERVATION MEASURES (1 OF 2)
- C11 HIP 4 GENERAL CONSERVATION MEASURES (2 OF 2)

PROJECT DESCRIPTION

THESE DRAWINGS PROVIDE 100% DESIGN LEVEL DETAILS FOR A FISH PASSAGE IMPROVEMENT PROJECT IN THE BRUSH CREEK WATERSHED LOCATED WITHIN YAKAMA TRIBAL LAND IN YAKIMA COUNTY, WASHINGTON.

THE BRUSH CREEK FISH PASSAGE AT 175 ROAD PROJECT WILL REPLACE THREE PARALLEL CULVERTS AND CONCRETE ARMORING WITH A BRIDGE TO IMPROVE FISH PASSAGE CONDITIONS ALONG BRUSH CREEK.

WORK SHALL CONSIST OF REMOVAL AND DISPOSAL OF THE EXISTING CROSSINGS AND REPLACEMENT WITH BRIDGE SPANS. THE NEW CREEK BED WILL BE CONSTRUCTED OF NATURAL STREAMBED MATERIAL.

ABBREVIATIONS

AVG.	AVERAGE	RC	RELATIVE COMPACTION
CC	CONCRETE	RSP	ROCK SLOPE PROTECTION
CY	CUBIC YARDS	SPK	SPIKE
DIA.	DIAMETER	SQ.FT.	SQUARE FOOT
E	EXISTING	T	TREE
EG	EXISTING GROUND	T.B.D.	TO BE DETERMINED
ELEV.	ELEVATION	TYP	TYPICAL
DI	DRAINAGE INLET	UNK	UNKNOWN
FG	FINISHED GRADE	WSE	WATER SURFACE
FT	FEET	ELEVATION	ELEVATION
INV	INVERT	YR	YEAR
MIN	MINIMUM		
N	NEW		
NIC	NOT IN CONTRACT	<u>TREE SPECIES</u>	
N.T.S.	NOT TO SCALE	CW	COTTONWOOD
O.C.	ON CENTER	P	PINE

SECTION AND DETAIL CONVENTION

SECTION OR DETAIL IDENTIFICATION
(NUMBER OR LETTER)



SHEET REFERENCE

GENERAL NOTES

- TOPOGRAPHIC MAPPING WAS PERFORMED BY: WATERWAYS CONSULTING, INC. 1020 SW TAYLOR STREET, SUITE 380 PORTLAND, OR 97205 SURVEY DATE: AUGUST 25, 2020.
- ELEVATION DATUM: AN ASSUMED ELEVATION OF 2,758.75' WAS ESTABLISHED AT SURVEY CONTROL POINT #4 (REBAR) SHOWN ON SHEET C2.
- BASIS OF BEARINGS: GPS TIES TO NAD83 WASHINGTON STATE PLANE SOUTH US FOOT USING THE LEICA GEOSYSTEMS SMARTNET GLOBAL NAVIGATION SATELLITE SYSTEM (GNSS) NETWORK.
- CONTOUR INTERVAL IS ONE FOOT. ELEVATIONS AND DISTANCES SHOWN ARE IN DECIMAL FEET.
- THIS IS NOT A BOUNDARY SURVEY. PROPERTY LINES WERE COMPILED FROM RECORD INFORMATION. THE LOCATION OF THESE LINES IS SUBJECT TO CHANGE, PENDING THE RESULTS OF A COMPLETE BOUNDARY SURVEY.
- ALL CONSTRUCTION AND MATERIALS SHALL CONFORM TO THE 2021 STANDARD SPECIFICATIONS FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION, ISSUED BY THE WASHINGTON STATE DEPARTMENT OF TRANSPORTATION (HEREAFTER REFERRED TO AS "STANDARD SPECIFICATIONS").
- THESE DESIGNS ARE INCOMPLETE WITHOUT THE FINAL STAMPED TECHNICAL SPECIFICATIONS PREPARED BY WATERWAYS CONSULTING, INC. REFER TO TECHNICAL SPECIFICATIONS FOR DETAILS NOT SHOWN HEREON.

*** CALL BEFORE YOU DIG ***
CONTACT UNDERGROUND SERVICE ALERT (USA)
PRIOR TO ANY CONSTRUCTION WORK 1-800-424-5555

REV.	DATE	DESCRIPTION	BY
1	12/4/24	NO CHANGES THIS SHEET	M.M.
2	9/11/25	NO CHANGES THIS SHEET	M.M.

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Jake D. Hofeld
JAKE D. HOFELD

PREPARED AT THE REQUEST OF:
YAKAMA NATION FISHERIES

COVER SHEET

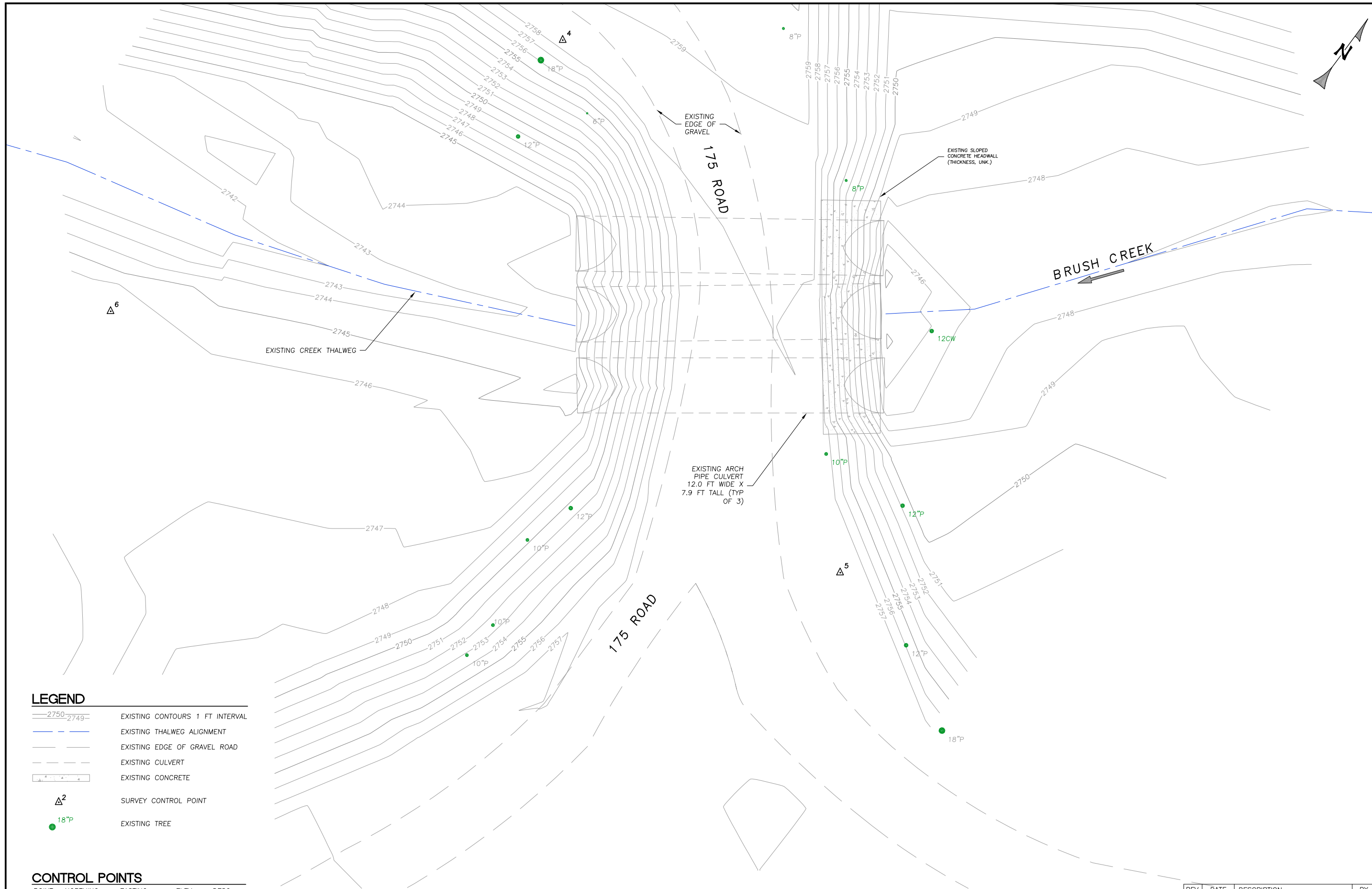
BRUSH CREEK FISH PASSAGE AT 175 ROAD 100% DESIGN

DESIGNED BY: J.H.
DRAWN BY: D.H./M.M.
CHECKED BY: J.H.
DATE: 9/11/2025
JOB NO.: 20-026

BAR IS ONE INCH ON ORIGINAL DRAWING, ADJUST SCALES FOR REDUCED PLOTS

0 1" 1" OF 11

F:\Projects\20-026 Cedar Valley Road Crossings\CAD\0-Brush Creek\BRUSH CREEK EXISTING CONDITIONS.dwg - 9/11/2025 4:31 PM



LEGEND

- EXISTING CONTOURS 1 FT INTERVAL
- EXISTING THALWEG ALIGNMENT
- EXISTING EDGE OF GRAVEL ROAD
- EXISTING CULVERT
- EXISTING CONCRETE
- SURVEY CONTROL POINT
- EXISTING TREE

CONTROL POINTS

POINT	NORTHING	EASTING	ELEV.	DESC.
4	283697.62'	1508041.08'	2758.75'	REBAR
5	283640.27'	1508158.46'	2757.93'	REBAR
6	283591.72'	1507996.92'	2747.10'	REBAR

EXISTING CONDITIONS
SCALE: 1" = 10'

REV.	DATE	DESCRIPTION	BY
△	12/4/24	NO CHANGES THIS SHEET	M.M.
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Jake D. Hofeld
 JAKE D. HOFELD
 EXPIRES: 4/9/2026

PREPARED AT THE REQUEST OF:
YAKAMA NATION FISHERIES

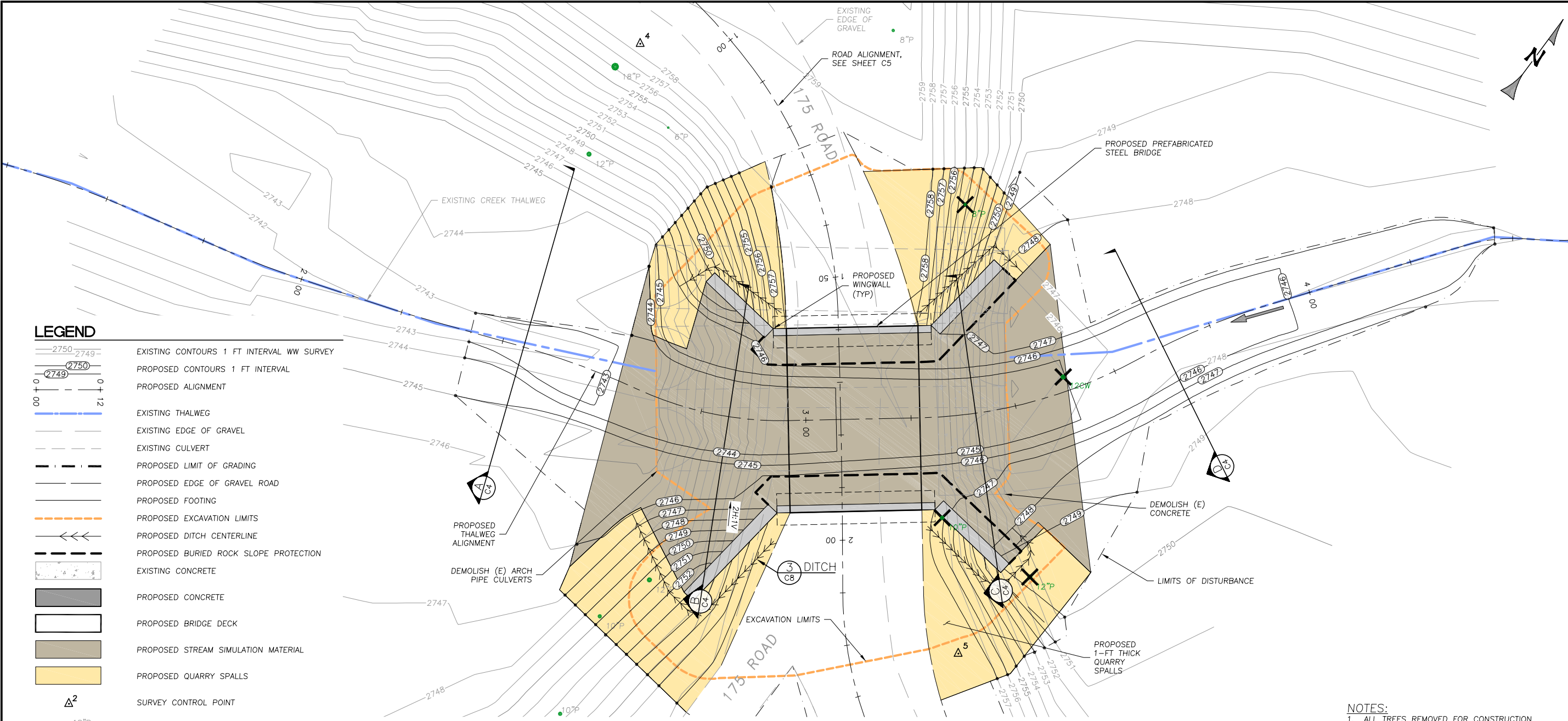
EXISTING CONDITIONS

BRUSH CREEK FISH PASSAGE AT 175 ROAD 100% DESIGN

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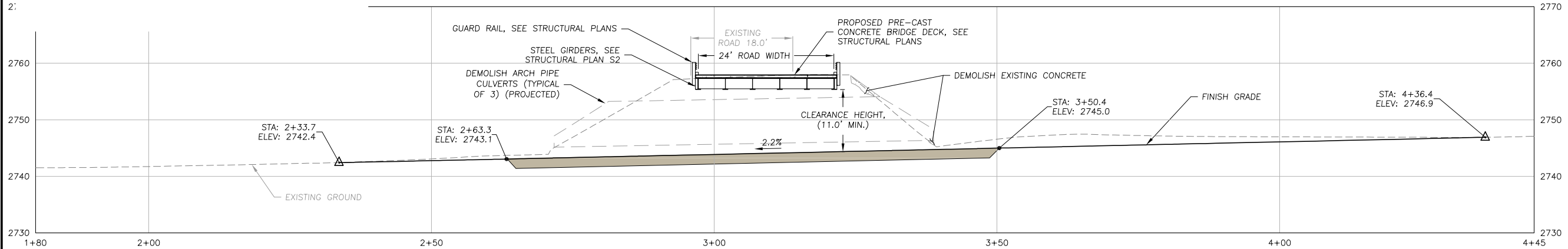


LEGEND

	EXISTING CONTOURS 1 FT INTERVAL WW SURVEY
	PROPOSED CONTOURS 1 FT INTERVAL
	PROPOSED ALIGNMENT
	EXISTING THALWEG
	EXISTING EDGE OF GRAVEL
	EXISTING CULVERT
	PROPOSED LIMIT OF GRADING
	PROPOSED EDGE OF GRAVEL ROAD
	PROPOSED FOOTING
	PROPOSED EXCAVATION LIMITS
	PROPOSED DITCH CENTERLINE
	PROPOSED BURIED ROCK SLOPE PROTECTION
	EXISTING CONCRETE
	PROPOSED CONCRETE
	PROPOSED BRIDGE DECK
	PROPOSED STREAM SIMULATION MATERIAL
	PROPOSED QUARRY SPALLS
	SURVEY CONTROL POINT
	EXISTING TREE
	EXISTING TREE TO BE REMOVED

CREEK PLAN
SCALE: 1" = 10'

NOTES:
1. ALL TREES REMOVED FOR CONSTRUCTION SHALL BE SALVAGED AS WHOLE TREES WITH ROOT WADS ATTACHED, AND SHALL BE PLACED IN THE FLOODPLAIN DOWNSTREAM OF THE BRIDGE AT THE DIRECTION OF THE ENGINEER.



BRUSH CREEK THALWEG PROFILE
SCALE: 1" = 10'

REV.	DATE	DESCRIPTION	BY
△	12/4/24	NO CHANGES THIS SHEET	M.M.
△	9/8/25	UPDATED STRUCTURE	M.M.

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Professional Engineer
No. 12345
Expires: 4/9/2026

PREPARED AT THE REQUEST OF:
YAKAMA NATION FISHERIES

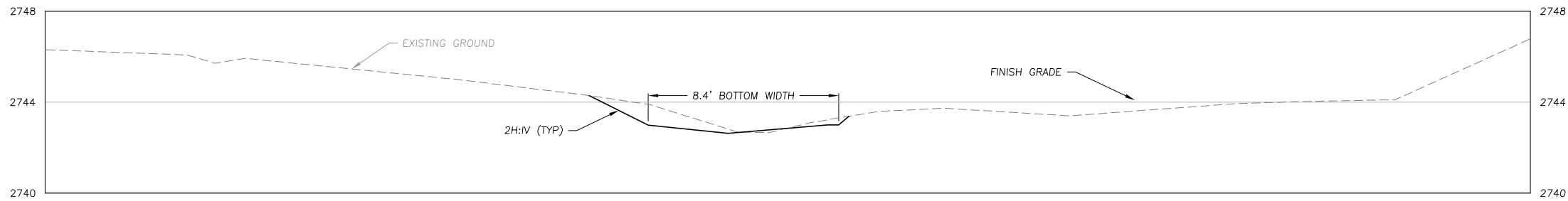
CREEK PLAN

BRUSH CREEK FISH PASSAGE AT 175 ROAD 100% DESIGN

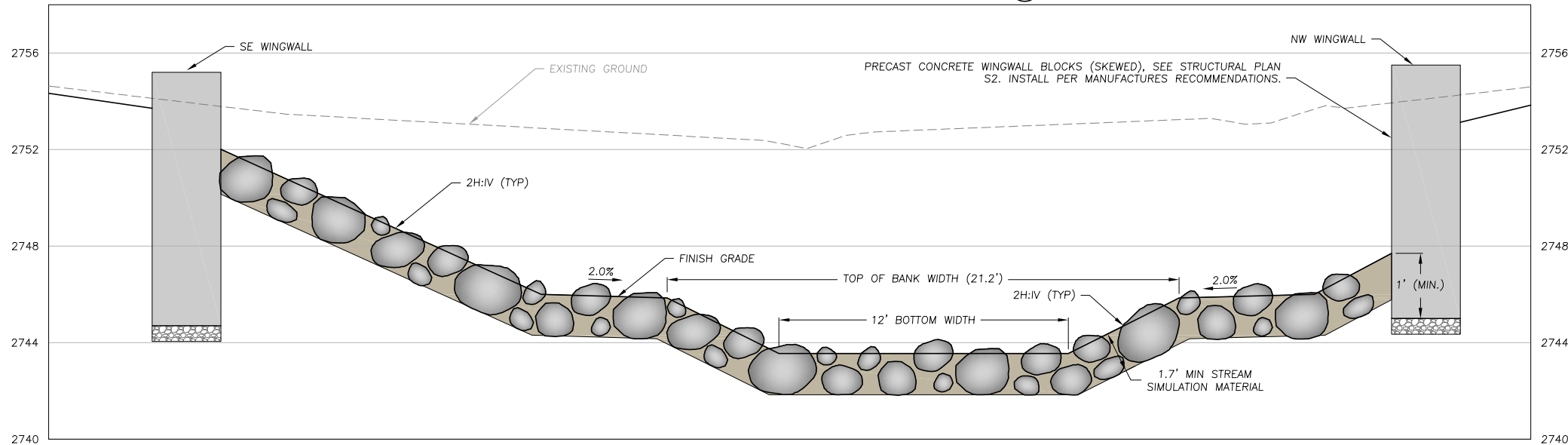
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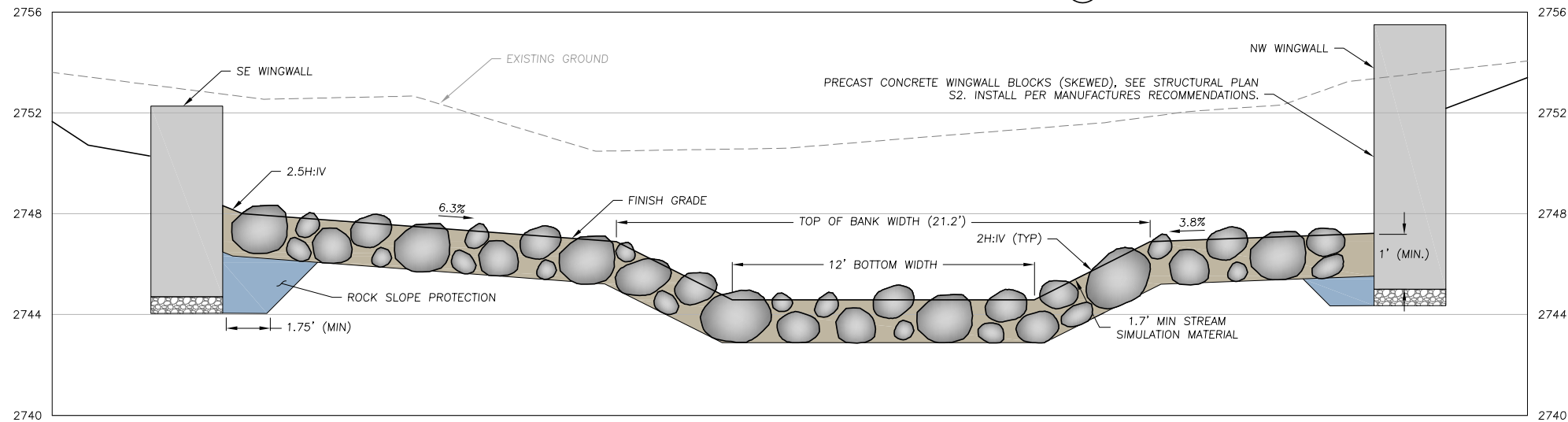
C3 3 OF 11



CREEK SECTION DOWNSTREAM OF CROSSING (A)
SCALE: 1" = 3'



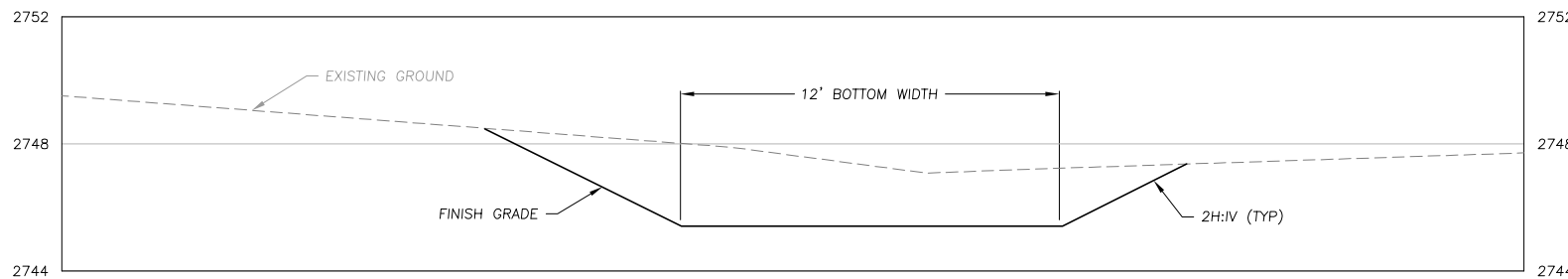
DOWNSTREAM STREAM SIMULATION MATERIAL SECTION (B)
SCALE: 1" = 3'



UPSTREAM STREAM SIMULATION MATERIAL SECTION (C)
SCALE: 1" = 3'

BRUSH CREEK STREAM SIMULATION MATERIAL GRADUATION	
SIZE CLASS	PARTICLE DIAMETER (FT)
D100 =	1.9
D75 =	0.75
D50 =	0.4
D25 =	0.21

NOTES:
NO GRADING OF THE CHANNEL PROPOSED OUTSIDE OF THE ROAD FILL PRISM.



CREEK SECTION UPSTREAM OF CROSSING (D)
SCALE: 1" = 3'

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△	12/4/24	UPDATED ABUTMENTS	M.M.
△	9/11/25	UPDATED STRUCTURE	M.M.

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Professional Engineer
JAKE D. HOFELD
EXPIRES: 4/9/2026

PREPARED AT THE REQUEST OF:
YAKAMA NATION FISHERIES

CREEK SECTIONS

BRUSH CREEK FISH PASSAGE AT 175 ROAD 100% DESIGN

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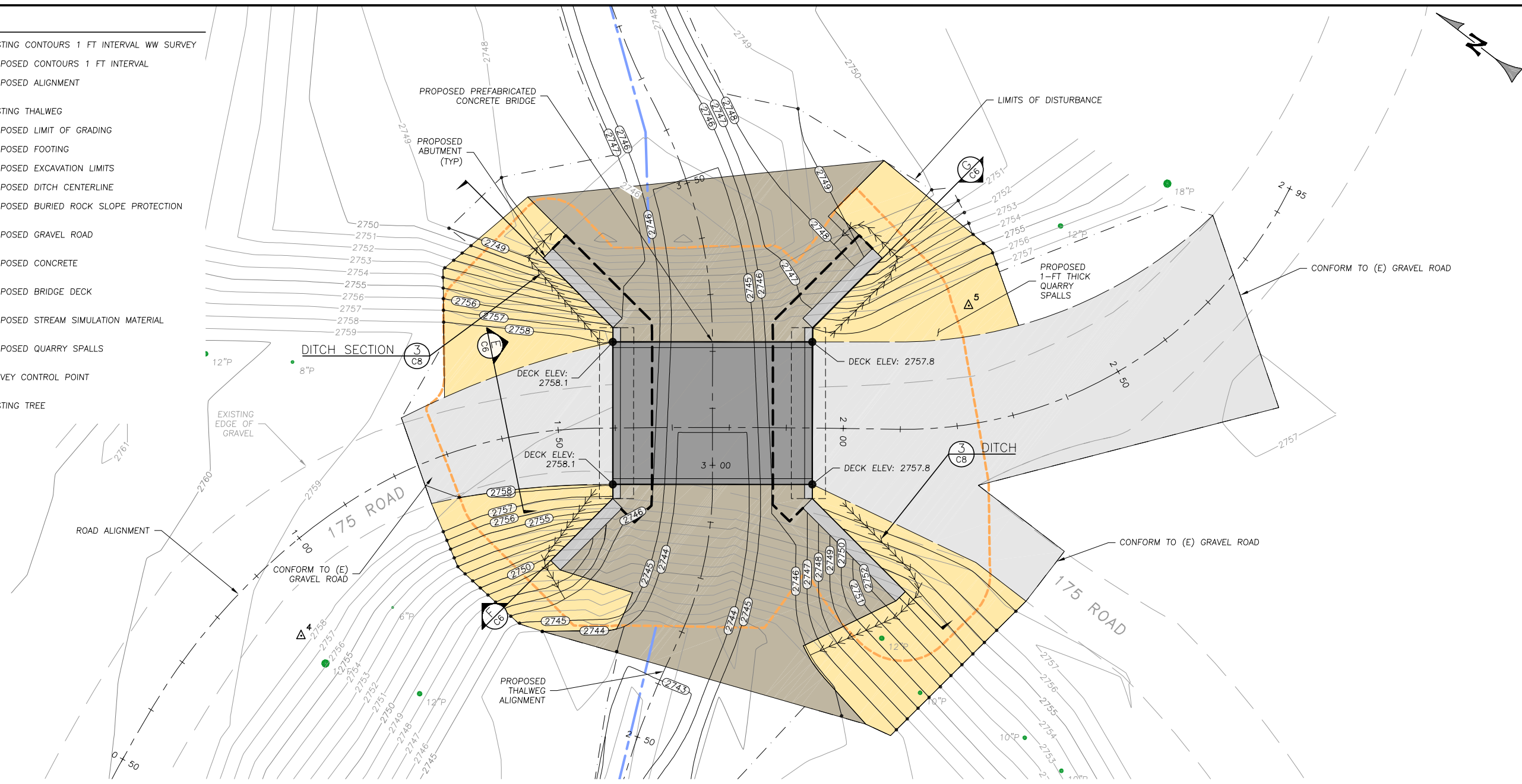
C4
4 OF 11

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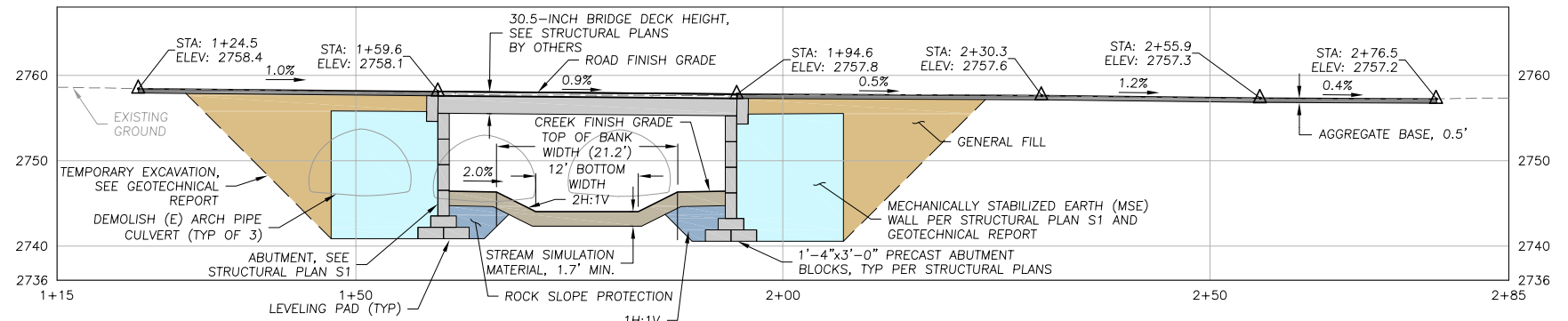
LEGEND

	EXISTING CONTOURS 1 FT INTERVAL WW SURVEY
	PROPOSED CONTOURS 1 FT INTERVAL
	PROPOSED ALIGNMENT
	EXISTING THALWEG
	PROPOSED LIMIT OF GRADING
	PROPOSED FOOTING
	PROPOSED EXCAVATION LIMITS
	PROPOSED DITCH CENTERLINE
	PROPOSED BURIED ROCK SLOPE PROTECTION
	PROPOSED GRAVEL ROAD
	PROPOSED CONCRETE
	PROPOSED BRIDGE DECK
	PROPOSED STREAM SIMULATION MATERIAL
	PROPOSED QUARRY SPALLS
	SURVEY CONTROL POINT
	EXISTING TREE



ROAD PLAN
SCALE: 1" = 10'

NOTES:
1. ALL TREES REMOVED FOR CONSTRUCTION SHALL BE SALVAGED AS WHOLE TREES WITH ROOT WADS ATTACHED, AND SHALL BE PLACED IN THE FLOODPLAIN DOWNSTREAM OF THE BRIDGE AT THE DIRECTION OF THE ENGINEER.



ROAD PROFILE
SCALE: 1" = 10'

REV.	DATE	DESCRIPTION	BY
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△	9/11/25	UPDATED STRUCTURE	M.M.

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James D. Hofeld
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EXPIRES: 4/9/2026

PREPARED AT THE REQUEST OF:
YAKAMA NATION FISHERIES

ROAD PLAN

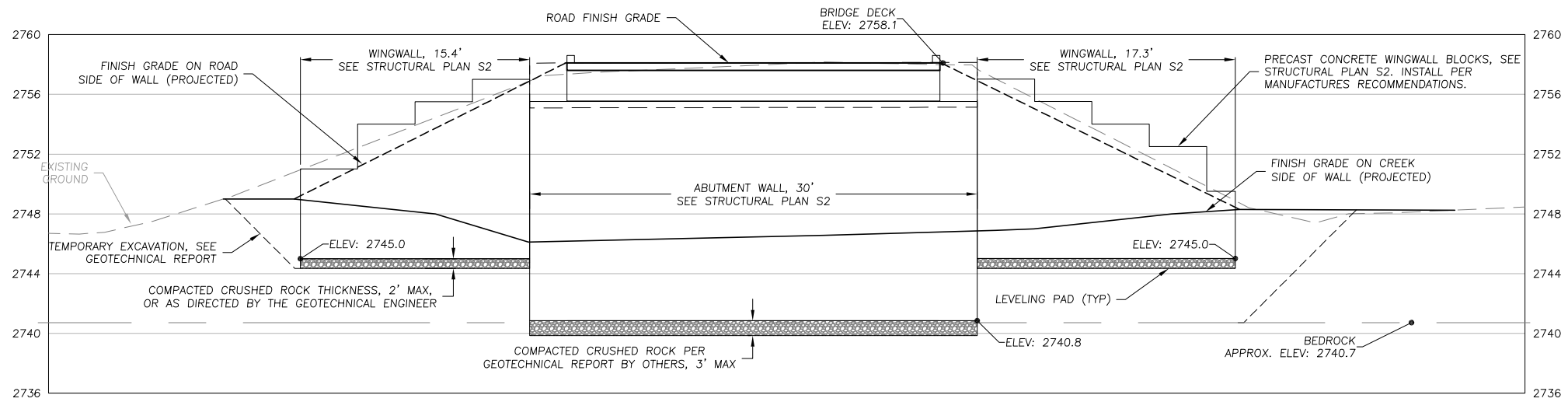
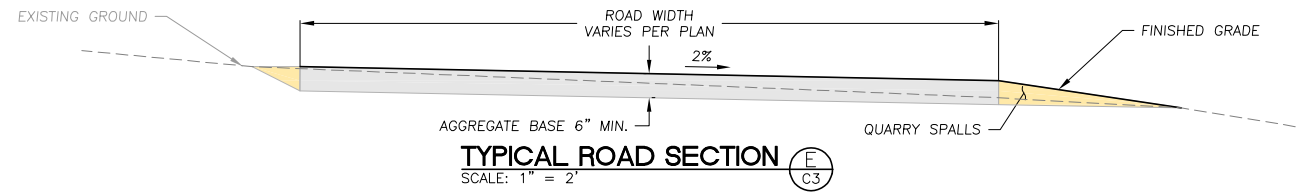
BRUSH CREEK FISH PASSAGE AT 175 ROAD
100% DESIGN

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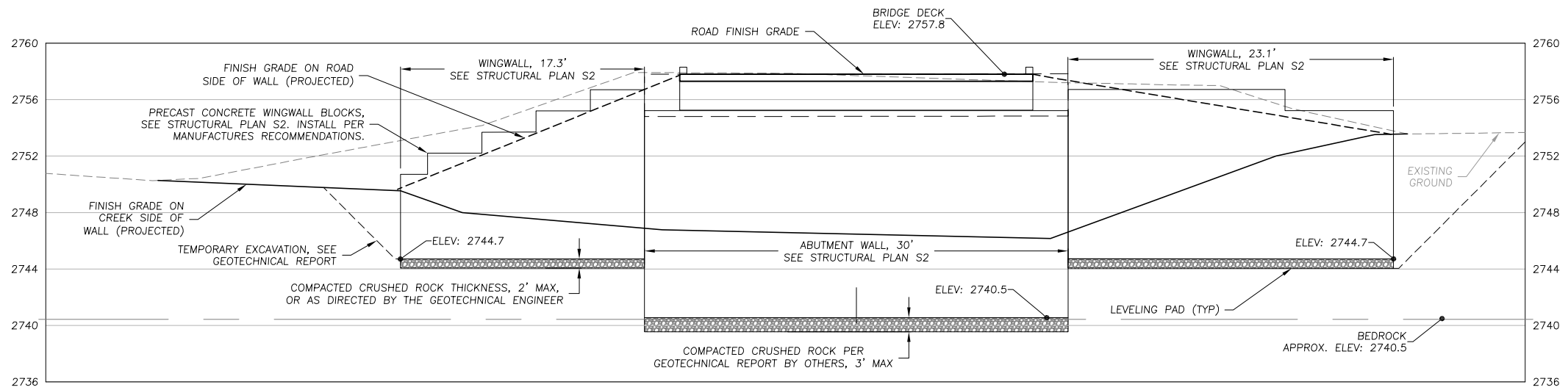
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C5 5 OF 11

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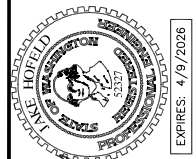


NORTHWEST ABUTMENT SECTION
SCALE: 1" = 5'



SOUTHEAST ABUTMENT SECTION
SCALE: 1" = 5'

REV.	DATE	DESCRIPTION	BY
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2	9/11/25	UPDATED STRUCTURE	M.M.



Jake D. Hofeld
JAKE D. HOFELD

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ROAD PROFILE AND ELEVATIONS

BRUSH CREEK FISH PASSAGE AT 175 ROAD 100% DESIGN

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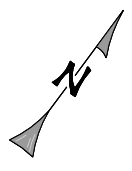
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APPROXIMATE LOCATION OF REFUELING AREA. REFUELING AREA MUST BE A MINIMUM OF 150-FT FROM BRUSH CREEK.

CONSTRUCTION SEQUENCING NOTES:

1. FLAG THE FOLLOWING LOCATIONS PRIOR TO CONSTRUCTION.
 - a. ORDINARY HIGH WATER
 - b. ROAD AND STREAM CROSSING ALIGNMENTS
 - c. STAGING, STORAGE AND STOCKPILE AREAS
 - d. LIMITS OF CONSTRUCTION
 - e. TREES AND OTHER VEGETATION WITHIN THE LIMITS OF CONSTRUCTION TO BE PROTECTED.
2. INSTALL TEMPORARY EROSION CONTROL BMPs AROUND THE DOWNSTREAM PERIMETER OF THE STAGING, STORAGE, STOCKPILE AREAS, AND LIMITS OF CONSTRUCTION.
3. IMPLEMENT THE FOLLOWING DEWATERING AND FISH SALVAGE MEASURES PRIOR TO IN-WATER CONSTRUCTION IF WATER IS PRESENT WITHIN THE CHANNEL.
 - a. ISOLATE UPSTREAM AND DOWNSTREAM ENDS OF THE CHANNEL WITH FISH BLOCK NETS AND PERFORM FISH SALVAGE OPERATIONS.
 - b. INSTALL A SAND BAG BERM AT THE UPSTREAM END OF THE WORK AREA AND DIVERT CREEK FLOW AROUND THE WORK AREA USING A PUMP WITH NMFS APPROVED PUMP IF FLOW IS BELOW 3 CFS, AND WITH A GRAVITY FLOW PIPE IF 3 CS OR GREATER.
- c. ALL WATER GENERATED FROM DEWATERING ACTIVITIES WITHIN THE WORK AREA SHALL BE PUMPED TO AN UPLAND LOCATION FOR INFILTRATION OR TREATMENT TO WATER QUALITY STANDARDS BEFORE FLOWING BACK INTO THE CREEK.
- d. MAINTAIN ALL WATER QUALITY INSPECTION REPORTS IN ACCORDANCE WITH PERMITS AND SUBMIT PRIOR TO FINAL PAYMENT.
4. EXCAVATE FOR BRIDGE FOOTINGS AND DEMOLISH EXISTING CULVERTS.
5. INSTALL BRIDGE FOOTINGS, ROCK SLOPE PROTECTION, AND STREAM SUBSTRATE MATERIAL.
6. INSTALL BRIDGE DECK AND APPROACH ROAD FILL.
7. INSTALL PERMANENT EROSION CONTROL MEASURES ON SIDE SLOPES OF ROAD FILL AND REMOVE DEWATERING EQUIPMENT.
8. SEED AND MULCH ALL DISTURBED SOILS OUTSIDE THE NEW ROAD AND STREAMBED SURFACES.
9. DEMOBILIZE FROM THE SITE AND RESTORE THE ACCESS TO PRECONSTRUCTION CONDITIONS.



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Jake D. Hofeld
 JAKE D. HOFELD

PREPARED AT THE REQUEST OF:
YAKAMA NATION FISHERIES

EROSION CONTROL AND DEWATERING PLAN

BRUSH CREEK FISH PASSAGE AT 175 ROAD
 100% DESIGN

DESIGNED BY: J.H.
 DRAWN BY: D.H./M.M.
 CHECKED BY: J.H.
 DATE: 9/11/2025
 JOB NO.: 20-026

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C7 7 OF 11

EROSION CONTROL AND DEWATERING PLAN

SCALE: 1" = 20'

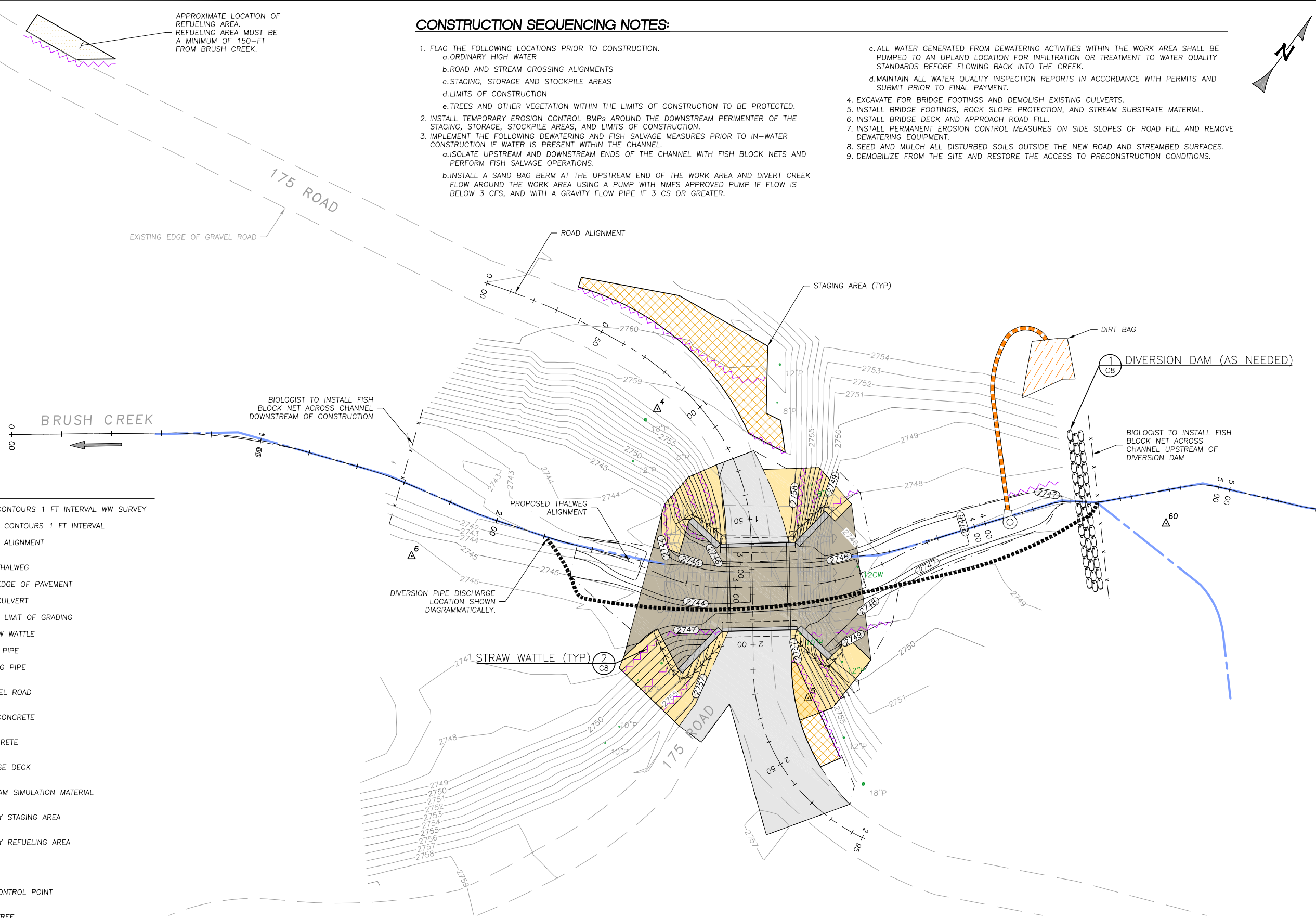
NOTES:

1. EQUIPMENT REFUELING AREA MUST BE A MINIMUM OF 150-FT FROM BRUSH CREEK.
2. PLACE STRAW WATTLES IN LINE WITH FINISHED GRADE CONTOURS.

REV.	DATE	DESCRIPTION	BY
1	12/4/24	NO CHANGES THIS SHEET	M.M.
2	9/11/25	UPDATED STRUCTURE	M.M.

LEGEND

- EXISTING CONTOURS 1 FT INTERVAL WW SURVEY
- PROPOSED CONTOURS 1 FT INTERVAL
- PROPOSED ALIGNMENT
- EXISTING THALWEG
- EXISTING EDGE OF PAVEMENT
- EXISTING CULVERT
- PROPOSED LIMIT OF GRADING
- NEW STRAW WATTLE
- DIVERSION PIPE
- DEWATERING PIPE
- NEW GRAVEL ROAD
- EXISTING CONCRETE
- NEW CONCRETE
- NEW BRIDGE DECK
- NEW STREAM SIMULATION MATERIAL
- TEMPORARY STAGING AREA
- TEMPORARY REFUELING AREA
- DIRT BAG
- SURVEY CONTROL POINT
- EXISTING TREE
- GROUNDWATER DEWATERING WELL/PUMP



BIOLOGIST TO INSTALL FISH BLOCK NET ACROSS CHANNEL DOWNSTREAM OF CONSTRUCTION

1 DIVERSION DAM (AS NEEDED)

BIOLOGIST TO INSTALL FISH BLOCK NET ACROSS CHANNEL UPSTREAM OF DIVERSION DAM

DIVERSION PIPE DISCHARGE LOCATION SHOWN DIAGRAMMATICALLY.

STRAW WATTLE (TYP)

ROAD ALIGNMENT

STAGING AREA (TYP)

DIRT BAG

BRUSH CREEK

175 ROAD

EXISTING EDGE OF GRAVEL ROAD

PROPOSED THALWEG ALIGNMENT

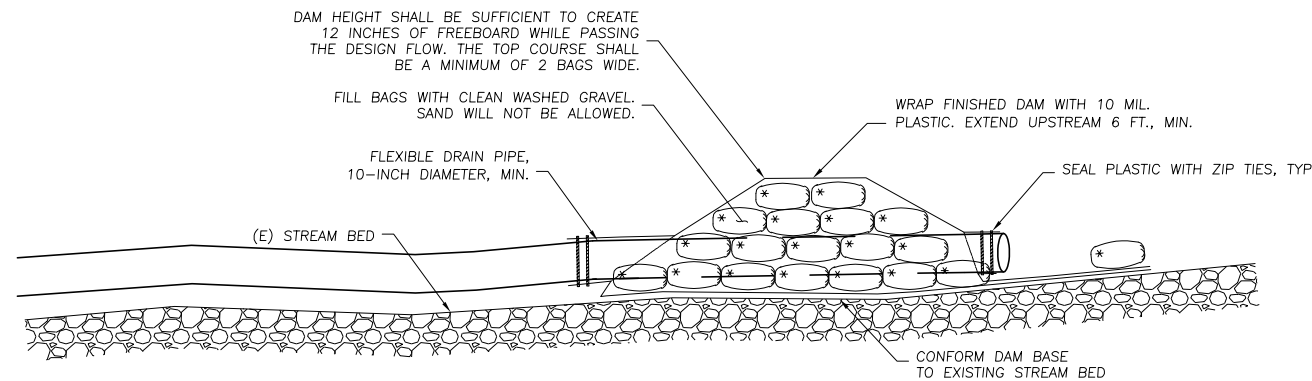
175 ROAD

EROSION CONTROL NOTES

1. THE EROSION CONTROL MEASURES SHOWN ON THIS PLAN ARE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, UPGRADE THESE MEASURES AS NEEDED TO COMPLY WITH ALL APPLICABLE LOCAL, STATE, AND FEDERAL EROSION AND SEDIMENT CONTROL REGULATIONS.
2. PHASE CLEARING AND GRADING TO THE MAXIMUM EXTENT PRACTICAL TO PREVENT EXPOSED INACTIVE AREAS FROM BECOMING A SOURCE OF EROSION.
3. IDENTIFY, MARK, AND PROTECT (BY FENCING OFF OR OTHER MEANS) CRITICAL RIPARIAN AREAS AND VEGETATION INCLUDING IMPORTANT TREES AND ASSOCIATED ROOTING ZONES, AND VEGETATION AREAS TO BE PRESERVED. IDENTIFY VEGETATIVE BUFFER ZONES BETWEEN THE SITE AND SENSITIVE AREAS (E.G., WETLANDS), AND OTHER AREAS TO BE PRESERVED, ESPECIALLY IN PERIMETER AREAS.
4. PRESERVE EXISTING VEGETATION WHEN PRACTICAL AND RE-VEGETATE OPEN AREAS. RE-VEGETATE OPEN AREAS WHEN PRACTICABLE BEFORE AND AFTER GRADING OR CONSTRUCTION. IDENTIFY THE TYPE OF VEGETATIVE SEED MIX USED.
5. EROSION AND SEDIMENT CONTROL MEASURES INCLUDING PERIMETER SEDIMENT CONTROL MUST BE IN PLACE BEFORE VEGETATION IS DISTURBED AND MUST REMAIN IN PLACE AND BE MAINTAINED, REPAIRED, AND PROMPTLY IMPLEMENTED FOLLOWING PROCEDURES ESTABLISHED FOR THE DURATION OF CONSTRUCTION, INCLUDING PROTECTION FOR ACTIVE STORM DRAIN INLETS AND CATCH BASINS AND APPROPRIATE NON-STORMWATER POLLUTION CONTROLS.
6. APPLY TEMPORARY AND/OR PERMANENT SOIL STABILIZATION MEASURES IMMEDIATELY ON ALL DISTURBED AREAS AS GRADING PROGRESSES AND FOR ALL ROADWAYS INCLUDING GRAVEL ROADWAYS.
7. ESTABLISH MATERIAL AND WASTE STORAGE AREAS, AND OTHER NON-STORMWATER CONTROLS.
8. PREVENT TRACKING OF SEDIMENT ONTO PUBLIC OR PRIVATE ROADS USING BMPs SUCH AS: GRAVELED (OR PAVED) EXITS AND PARKING AREAS, GRAVEL ALL UNPAVED ROADS LOCATED ONSITE, OR USE AN EXIT TIRE WASH. THESE BMPs MUST BE IN PLACE PRIOR TO LAND-DISTURBING ACTIVITIES.
9. WHEN TRUCKING SATURATED SOILS FROM THE SITE, EITHER USE WATER-TIGHT TRUCKS OR DRAIN LOADS ON SITE.

10. USE BMPs TO PREVENT OR MINIMIZE STORMWATER EXPOSURE TO POLLUTANTS FROM SPILLS; VEHICLE AND EQUIPMENT FUELING, MAINTENANCE, AND STORAGE; OTHER CLEANING AND MAINTENANCE ACTIVITIES; AND WASTE HANDLING ACTIVITIES. THESE POLLUTANTS INCLUDE FUEL, HYDRAULIC FLUID, AND OTHER OILS FROM VEHICLES AND MACHINERY, AS WELL AS DEBRIS, LEFTOVER PAINTS, SOLVENTS, AND GLUES FROM CONSTRUCTION OPERATIONS.
11. FUELING ACTIVITIES MUST BE LOCATED A MINIMUM OF 150 FEET FROM ORDINARY HIGH WATER AND SENSITIVE WATERS, INCLUDING WETLANDS.
12. IMPLEMENT THE FOLLOWING BMPs WHEN APPLICABLE: WRITTEN SPILL PREVENTION AND RESPONSE PROCEDURES, EMPLOYEE TRAINING ON SPILL PREVENTION AND PROPER DISPOSAL PROCEDURES, SPILL KITS IN ALL VEHICLES, REGULAR MAINTENANCE SCHEDULE FOR VEHICLES AND MACHINERY, MATERIAL DELIVERY AND STORAGE CONTROLS, TRAINING AND SIGNAGE, AND COVERED STORAGE AREAS FOR WASTE AND SUPPLIES.
13. USE WATER, SOIL-BINDING AGENT OR OTHER DUST CONTROL TECHNIQUE AS NEEDED TO AVOID WIND-BLOWN SOIL.
14. ONSITE VEHICLE SPEED ON UNPAVED SURFACES SHALL BE LIMITED TO 15 MPH.
15. THE APPLICATION RATE OF FERTILIZERS USED TO REESTABLISH VEGETATION MUST FOLLOW MANUFACTURER'S RECOMMENDATIONS TO MINIMIZE NUTRIENT RELEASES TO SURFACE WATERS. EXERCISE CAUTION WHEN USING TIME-RELEASE FERTILIZERS WITHIN ANY WATERWAY RIPARIAN ZONE.
16. IF A STORMWATER TREATMENT SYSTEM (FOR EXAMPLE, ELECTRO-COAGULATION, FLOCCULATION, FILTRATION, ETC.) FOR SEDIMENT OR OTHER POLLUTANT REMOVAL IS EMPLOYED, SUBMIT AN OPERATION AND MAINTENANCE PLAN (INCLUDING SYSTEM SCHEMATIC, LOCATION OF SYSTEM, LOCATION OF INLET, LOCATION OF DISCHARGE, DISCHARGE DISPERSION DEVICE DESIGN, AND A SAMPLING PLAN AND FREQUENCY) BEFORE OPERATING THE TREATMENT SYSTEM. OBTAIN PLAN APPROVAL BEFORE OPERATING THE TREATMENT SYSTEM. OPERATE AND MAINTAIN THE TREATMENT SYSTEM ACCORDING TO MANUFACTURER'S SPECIFICATIONS.
17. TEMPORARILY STABILIZE SOILS AT THE END OF THE SHIFT BEFORE HOLIDAYS AND WEEKENDS, IF NEEDED. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT SOILS ARE STABLE DURING RAIN EVENTS AT ALL TIMES OF THE YEAR.

18. AT THE END OF EACH WORKDAY SOIL STOCKPILES MUST BE STABILIZED OR COVERED, OR OTHER BMPs MUST BE IMPLEMENTED TO PREVENT DISCHARGES TO SURFACE WATERS OR CONVEYANCE SYSTEMS LEADING TO SURFACE WATERS.
19. CONSTRUCTION ACTIVITIES MUST AVOID OR MINIMIZE EXCAVATION AND CREATION OF BARE GROUND DURING WET WEATHER.
20. SEDIMENT FENCE: REMOVE TRAPPED SEDIMENT BEFORE IT REACHES ONE THIRD OF THE ABOVE GROUND FENCE HEIGHT AND BEFORE FENCE REMOVAL.
21. WITHIN 24 HOURS, SIGNIFICANT SEDIMENT THAT HAS LEFT THE CONSTRUCTION SITE, MUST BE REMEDIATED. INVESTIGATE THE CAUSE OF THE SEDIMENT RELEASE AND IMPLEMENT STEPS TO PREVENT A RECURRENCE OF THE DISCHARGE WITHIN THE SAME 24 HOURS. ANY IN-STREAM CLEAN UP OF SEDIMENT SHALL BE PERFORMED ACCORDING TO THE OREGON DIVISION OF STATE LANDS REQUIRED TIMEFRAME.
22. THE INTENTIONAL WASHING OF SEDIMENT INTO STORM SEWERS, DRAINAGE WAYS, OR WETLANDS MUST NOT OCCUR. VACUUMING OR DRY SWEEPING AND MATERIAL PICKUP MUST BE USED TO CLEANUP RELEASED SEDIMENTS.
23. THE ENTIRE SITE MUST BE TEMPORARILY STABILIZED USING VEGETATION OR A HEAVY MULCH LAYER, TEMPORARY SEEDING, OR OTHER METHOD SHOULD ALL CONSTRUCTION ACTIVITIES CEASE FOR 30 DAYS OR MORE.
24. PROVIDE TEMPORARY STABILIZATION FOR THAT PORTION OF THE SITE WHERE CONSTRUCTION ACTIVITIES CEASE FOR 14 DAYS OR MORE WITH A COVERING OF BLOWN STRAW AND A TACKIFIER, LOOSE STRAW, OR AN ADEQUATE COVERING OF COMPOST MULCH UNTIL WORK RESUMES ON THAT PORTION OF THE SITE.
25. PROVIDE PERMANENT EROSION CONTROL MEASURES ON ALL EXPOSED AREAS AS THEY ARE COMPLETED. DO NOT REMOVE TEMPORARY SEDIMENT CONTROL PRACTICES UNTIL PERMANENT VEGETATION OR OTHER COVER OF EXPOSED AREAS IS ESTABLISHED. HOWEVER, DO REMOVE ALL TEMPORARY EROSION CONTROL MEASURES AS EXPOSED AREAS BECOME STABILIZED, UNLESS DOING SO CONFLICTS WITH LOCAL REQUIREMENTS. PROPERLY DISPOSE OF CONSTRUCTION MATERIALS AND WASTE, INCLUDING SEDIMENT RETAINED BY TEMPORARY BMPs.



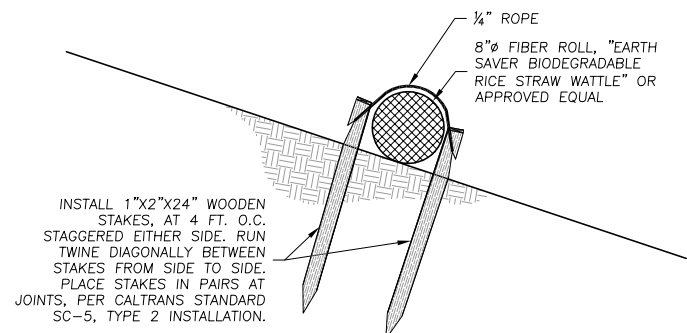
NOTE: CONTRACTOR MAY USE ALTERNATE DAM DETAIL, SUBJECT TO APPROVAL OF THE ENGINEER AND THE PERMITTING AGENCIES.

DIVERSION DAM PROFILE 1
SCALE: 1" = 5'

DIVERSION NOTES

THE DIVERSION PLAN SHOWN IS SCHEMATIC. GENERAL REQUIREMENTS ARE PROVIDED BELOW. THE FULL REQUIREMENTS OF THE DIVERSION AND DEWATERING PLAN ARE SPECIFIED IN THE PROJECT TECHNICAL SPECIFICATIONS.

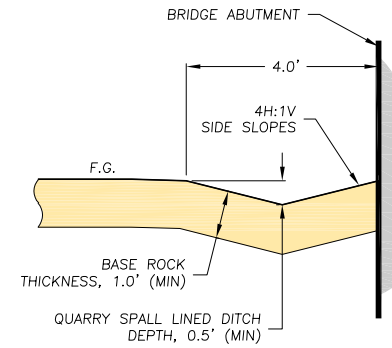
1. GENERAL
 - 1.1. DEWATER THE PROJECT SITE AS REQUIRED TO FACILITATE IN-STREAM CONSTRUCTION AND TO REDUCE POTENTIAL IMPACTS TO WATER QUALITY DOWNSTREAM OF THE PROJECT SITE.
 - 1.2. CONFIRM THAT A FAVORABLE LONG TERM WEATHER FORECAST (1 WEEK, MIN.) IS OBSERVED PRIOR TO PLACEMENT OF DIVERSION STRUCTURES.
 - 1.3. PRIOR TO PLACEMENT OF DIVERSION STRUCTURE, REMOVE FISH FROM THE PROJECT REACH, IN ACCORDANCE WITH SECTION 2.
 - 1.4. DIVERT FLOW ONLY WHEN THE DIVERSION CONSTRUCTION IS OTHERWISE COMPLETE. FOLLOWING ENGINEER'S APPROVAL OF THE COMPLETED WORK, REMOVE DIVERSION BEGINNING AT THE DOWNSTREAM LIMIT, IN AN UPSTREAM DIRECTION.
2. FISH REMOVAL
 - 2.1. FISH SHALL BE REMOVED FROM THE PROJECT SITE BY A QUALIFIED FISHERIES BIOLOGIST, AUTHORIZED TO PERFORM SUCH ACTIVITIES BY THE NATIONAL MARINE FISHERIES SERVICE AND THE WASHINGTON DEPARTMENT OF FISH AND WILDLIFE.
 - 2.2. BLOCK NETS SHALL BE PROVIDED AND INSTALLED BY THE FISHERIES BIOLOGIST. BLOCK NETS SHALL BE MAINTAINED BY THE CONTRACTOR BOTH UPSTREAM AND DOWNSTREAM OF THE DIVERSION, THROUGHOUT THE PERIOD OF CONSTRUCTION. MAINTENANCE INCLUDES PERIODIC REMOVAL OF ACCUMULATED DEBRIS, AS NECESSARY TO ENSURE FUNCTION. BLOCK NETS SHALL BE REMOVED BY THE FISHERIES BIOLOGIST AFTER THE DIVERSION IS REMOVED AND THE IN CHANNEL WORK AREA IS RE-WATERED.
3. DIVERSION SYSTEM
 - 3.1. INSTALL A SEALED, TEMPORARY DIVERSION DAM CONSTRUCTED USING GRAVEL FILLED BAGS TO CAPTURE AND DIVERT STREAM FLOW UPSTREAM OF THE PROJECT SITE. THE DAM AND METHOD OF SEALING SHALL BE PLACED AT AN APPROPRIATE DEPTH TO CAPTURE SUBSURFACE STREAM FLOW, AS NEEDED TO DEWATER THE STREAMBED. GRAVEL SHALL BE WASHED PRIOR TO PLACEMENT IN BAGS. THE USE OF SAND WILL NOT BE ALLOWED. NO OTHER DIVERSION METHOD SHALL BE USED WITHOUT AUTHORIZATION OF THE ENGINEER. IF AN ALTERNATE DIVERSION METHOD IS PREFERRED BY THE CONTRACTOR, THE CONTRACTOR SHALL SUBMIT A PLAN TO THE ENGINEER FOR APPROVAL, DETAILING THE DESIRED DIVERSION METHOD.
 - 3.2. THE DIVERSION STRUCTURE SHALL BE CONSTRUCTED AS SHOWN ON DETAIL 1 ON SHEET C8 OR AS DIRECTED BY THE ENGINEER IN THE FIELD.
 - 3.3. IN THE EVENT OF A SIGNIFICANT STORM, THE CONTRACTOR SHALL BE PREPARED TO TAKE NECESSARY MEASURES TO INSURE SAFE PASSAGE OF STORM WATER FLOW THROUGH THE PROJECT AREA, WITHOUT DAMAGE TO EXISTING STRUCTURES, OR INTRODUCTION OF EXCESSIVE SEDIMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TEMPORARY EROSION CONTROL BMPs.
 - 3.4. THE DIVERSION SHALL BE CAPABLE OF CONVEYING ANTICIPATED FLOWRATES WITH LESS THAN 6 INCHES OF HEAD OVER THE TOP OF PIPE AT THE INLET, AND SHALL BE A MINIMUM DIAMETER OF 10", WITH A MANNING'S ROUGHNESS NOT EXCEEDING .012.
4. DEWATERING OF CONSTRUCTION AREAS
 - 4.1. THE CONTRACTOR SHALL SUPPLY ALL NECESSARY PUMPS, PIPING, FILTERS, SHORING, AND OTHER TOOLS AND MATERIALS NECESSARY FOR DEWATERING. IF A PUMPED SYSTEM IS RELIED UPON TO ENSURE DOWNSTREAM WATER QUALITY, A BACKUP PUMP OF EQUAL CAPACITY SHALL BE PROVIDED AT ALL TIMES AND THE PUMP MUST BE CONTINUOUSLY MONITORED.
 - 4.2. DEWATERING ACTIVITIES WHICH MAY BE REQUIRED FOR CONSTRUCTION PURPOSES SHALL COMPLY WITH WATER QUALITY STANDARDS ISSUED BY WASHINGTON STATE DEPARTMENT OF ECOLOGY.
 - 4.3. DISCHARGE OF WATER FROM THE DEWATERED CONSTRUCTION SITE, EITHER BY GRAVITY OR PUMPING, SHALL BE PERFORMED IN A MANNER THAT PREVENTS EXCESSIVE TURBIDITY FROM ENTERING THE RECEIVING WATERWAYS AND PREVENTS SCOUR AND EROSION OUTSIDE OF THE CONSTRUCTION SITE. PUMPED WATER SHOULD BE PRE-FILTERED WITH A GRAVEL PACK AROUND SUMPS FOR SUBSURFACE FLOWS AND A SILT FENCE AROUND PUMPS FOR SURFACE FLOW. PUMPED WATER SHALL BE DISCHARGED INTO ISOLATED LOCAL DEPRESSIONS, FILTER BAGS, SETTLING (BAKER) TANKS, OR TEMPORARY SEDIMENT BASINS, AS NECESSARY TO MEET WATER QUALITY REQUIREMENTS. WHERE WATER TO BE DISCHARGED INTO BRUSH OR WHITE CREEK WILL CREATE EXCESSIVE TURBIDITY, THE WATER SHALL BE ROUTED THROUGH A SEDIMENT INTERCEPTOR OR OTHER FACILITIES TO REMOVE SEDIMENT FROM WATER.



STRAW WATTLE NOTES

1. CLEAR THE BEDDING AREA FOR THE STRAW WATTLE OF OBSTRUCTIONS INCLUDING ROCKS, CLODS, AND DEBRIS GREATER THAN ONE INCH IN DIAMETER BEFORE INSTALLATION.
2. CONSTRUCT FURROWS TO THE DEPTH SHOWN, AND TO A SUFFICIENT WIDTH TO HOLD THE FIBER ROLL. INSTALL STAKES AT THE ON-CENTER SPACING SHOWN ALONG THE LENGTH OF THE STRAW WATTLE AND STOPPED AT 12 INCHES FROM EACH END OF THE WATTLES. DRIVE STAKES TO BETWEEN TWO AND THREE INCHES ABOVE THE TOP OF THE ROLL.
3. PLACE STRAW WATTLES 10 FEET APART ALONG THE SLOPE FOR SLOPE INCLINATION OF 2H:1V AND STEEPER, AND 15 FEET APART ALONG THE SLOPE FOR SLOPE INCLINATION BETWEEN 2H:1V AND 4H:1V.
4. INSTALL STRAW WATTLES APPROXIMATELY PARALLEL TO THE SLOPE CONTOUR. ANGLE THE TERMINUS OF ROWS UP-SLOPE AT 45 DEGREES FOR A DISTANCE OF THREE FEET. WHERE FIBER ROLLS MEET, PROVIDE AN OVERLAP OF 18 INCHES, WITH ADJACENT ROLLS TIGHTLY ABUTTING EACH OTHER.
5. INSTALL STRAW WATTLES PRIOR TO SEEDING.

STRAW WATTLE DETAIL 2
SCALE: 1" = 1'



DITCH SECTION 3
SCALE: 1" = 2'

REV.	DATE	DESCRIPTION	BY
1	12/4/24	NO CHANGES THIS SHEET	M.M.
2	9/11/25	NO CHANGES THIS SHEET	M.M.

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JAKE D. HOFELD
4/9/2026

PREPARED AT THE REQUEST OF:
YAKAMA NATION FISHERIES

DETAILS AND NOTES

BRUSH CREEK FISH PASSAGE AT 175 ROAD 100% DESIGN

DESIGNED BY: J.H.
DRAWN BY: D.H./M.M.
CHECKED BY: J.H.
DATE: 9/11/2025
JOB NO.: 20-026

BAR IS ONE INCH ON ORIGINAL DRAWING, ADJUST SCALES FOR REDUCED PLOTS

GENERAL NOTES

1. NOTIFY THE ENGINEER AT LEAST 48 HOURS PRIOR TO CONSTRUCTION. THE ENGINEER OR A DESIGNATED REPRESENTATIVE SHALL OBSERVE THE CONSTRUCTION PROCESS, AS NECESSARY TO ENSURE PROPER INSTALLATION PROCEDURES.
2. EXISTING UNDERGROUND UTILITY LOCATIONS:
 - A. CALL UNDERGROUND SERVICE ALERT (1-800-424-5555) TO LOCATE ALL UNDERGROUND UTILITY LINES PRIOR TO COMMENCING CONSTRUCTION.
 - B. PRIOR TO BEGINNING WORK, CONTACT ALL UTILITIES COMPANIES WITH REGARD TO WORKING OVER, UNDER, OR AROUND EXISTING FACILITIES AND TO OBTAIN INFORMATION REGARDING RESTRICTIONS THAT ARE REQUIRED TO PREVENT DAMAGE TO THE FACILITIES.
 - C. EXISTING UTILITY LOCATIONS SHOWN ARE COMPILED FROM INFORMATION SUPPLIED BY THE APPROPRIATE UTILITY AGENCIES AND FROM FIELD MEASUREMENTS TO ABOVE GROUND FEATURES READILY VISIBLE AT THE TIME OF SURVEY. LOCATIONS SHOWN ARE APPROXIMATE. THE CONTRACTOR IS CAUTIONED THAT ONLY ACTUAL EXCAVATION WILL REVEAL THE DIMENSIONS, SIZES, MATERIALS, LOCATIONS, AND DEPTH OF UNDERGROUND UTILITIES.
 - D. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR THE LOCATION AND/OR PROTECTION OF ALL EXISTING AND PROPOSED PIPING, UTILITIES, TRAFFIC SIGNAL EQUIPMENT (BOTH ABOVE GROUND AND BELOW GROUND), STRUCTURES, AND ALL OTHER EXISTING IMPROVEMENTS THROUGHOUT CONSTRUCTION.
 - E. PRIOR TO COMMENCING FABRICATION OR CONSTRUCTION, DISCOVER OR VERIFY THE ACTUAL DIMENSIONS, SIZES, MATERIALS, LOCATIONS, AND ELEVATIONS OF ALL EXISTING UTILITIES AND POTHOLE THOSE AREAS WHERE POTENTIAL CONFLICTS ARE LIKELY OR DATA IS OTHERWISE INCOMPLETE.
 - F. TAKE APPROPRIATE MEASURES TO PROTECT EXISTING UTILITIES DURING CONSTRUCTION OPERATIONS. CONTRACTOR IS SOLELY RESPONSIBLE FOR THE COST OF REPAIR/REPLACEMENT OF ANY EXISTING UTILITIES DAMAGED DURING CONSTRUCTION.
 - G. ON LEARNING OF THE EXISTENCE AND/OR LOCATIONS OF ANY UNDERGROUND FACILITIES NOT SHOWN OR SHOWN INACCURATELY ON THE PLANS OR NOT PROPERLY MARKED BY THE UTILITY OWNER, IMMEDIATELY NOTIFY THE UTILITY OWNER AND THE CITY BY TELEPHONE AND IN WRITING.
 - H. UTILITY RELOCATIONS REQUIRED FOR THE CONSTRUCTION OF THE PROJECT FACILITIES WILL BE PERFORMED BY THE UTILITY COMPANY, UNLESS OTHERWISE NOTED.
3. IF DISCREPANCIES ARE DISCOVERED BETWEEN THE CONDITIONS EXISTING IN THE FIELD AND THE INFORMATION SHOWN ON THESE DRAWINGS, NOTIFY THE ENGINEER PRIOR TO PROCEEDING WITH CONSTRUCTION.
4. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO BE FULLY INFORMED OF AND TO COMPLY WITH ALL LAWS, ORDINANCES, CODES, REQUIREMENTS AND STANDARDS WHICH IN ANY MANNER AFFECT THE COURSE OF CONSTRUCTION OF THIS PROJECT, THOSE ENGAGED OR EMPLOYED IN THE CONSTRUCTION AND THE MATERIALS USED IN THE CONSTRUCTION.
5. ALL TESTS, INSPECTIONS, SPECIAL OR OTHERWISE, THAT ARE REQUIRED BY THE BUILDING CODES, LOCAL BUILDING DEPARTMENTS, OR THESE PLANS, SHALL BE DONE BY AN INDEPENDENT INSPECTION COMPANY. JOB SITE VISITS BY THE ENGINEER DO NOT CONSTITUTE AN OFFICIAL INSPECTION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT THE REQUIRED TESTS AND INSPECTIONS ARE PERFORMED.
6. PROJECT SCHEDULE: PRIOR TO COMMENCEMENT OF WORK, SUBMIT TO THE ENGINEER FOR REVIEW AND APPROVAL A DETAILED CONSTRUCTION SCHEDULE. DO NOT BEGIN ANY CONSTRUCTION WORK UNTIL THE PROJECT SCHEDULE AND WORK PLAN IS APPROVED BY THE ENGINEER. ALL CONSTRUCTION SHALL BE CLOSELY COORDINATED WITH THE ENGINEER SO THAT THE QUALITY OF WORK CAN BE CHECKED FOR APPROVAL. PURSUE WORK IN A CONTINUOUS AND DILIGENT MANNER TO ENSURE A TIMELY COMPLETION OF THE PROJECT.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DESIGN, PERMITTING, INSTALLATION, AND MAINTENANCE OF ANY AND ALL TRAFFIC CONTROL MEASURES DEEMED NECESSARY.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR GENERAL SAFETY DURING CONSTRUCTION. ALL WORK SHALL CONFORM TO PERTINENT SAFETY REGULATIONS AND CODES. THE CONTRACTOR SHALL BE SOLELY AND COMPLETELY RESPONSIBLE FOR FURNISHING, INSTALLING, AND MAINTAINING ALL WARNING SIGNS AND DEVICES NECESSARY TO SAFEGUARD THE GENERAL PUBLIC AND THE WORK, AND PROVIDE FOR THE PROPER AND SAFE ROUTING OF VEHICULAR AND PEDESTRIAN TRAFFIC DURING THE PERFORMANCE OF THE WORK. THE CONTRACTOR SHALL BE SOLELY AND COMPLETELY RESPONSIBLE FOR COMPLIANCE WITH ALL APPLICABLE PROVISIONS OF OSHA IN THE CONSTRUCTION PRACTICES FOR ALL EMPLOYEES DIRECTLY ENGAGED IN THE CONSTRUCTION OF THIS PROJECT.
9. CONSTRUCTION CONTRACTOR AGREES THAT IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, CONSTRUCTION CONTRACTOR WILL BE REQUIRED TO ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THE PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY; THAT THIS REQUIREMENT SHALL BE MADE TO APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS, AND CONSTRUCTION CONTRACTOR FURTHER AGREES TO DEFEND, INDEMNIFY AND HOLD DESIGN PROFESSIONAL HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTION LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF DESIGN PROFESSIONAL. NEITHER THE PROFESSIONAL ACTIVITIES OF CONSULTANT NOR THE PRESENCE OF CONSULTANT OR HIS OR HER EMPLOYEES OR SUB-CONSULTANTS AT A CONSTRUCTION SITE SHALL RELIEVE THE CONTRACTOR AND ITS SUBCONTRACTORS OF THEIR RESPONSIBILITIES INCLUDING, BUT NOT LIMITED TO, CONSTRUCTION MEANS, METHODS, SEQUENCE, TECHNIQUES OR PROCEDURES NECESSARY FOR PERFORMING, SUPERINTENDING OR COORDINATING ALL PORTIONS OF THE WORK OF CONSTRUCTION IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND APPLICABLE HEALTH OR SAFETY REQUIREMENTS OF ANY REGULATORY AGENCY OR OF STATE LAW.
10. MAINTAIN A CURRENT, COMPLETE, AND ACCURATE RECORD OF ALL AS-BUILT DEVIATIONS FROM THE CONSTRUCTION AS SHOWN ON THESE DRAWINGS AND SPECIFICATIONS, FOR THE PURPOSE OF PROVIDING THE ENGINEER OF RECORD WITH A BASIS FOR THE PREPARATION OF RECORD DRAWINGS.
11. MAINTAIN THE SITE IN A NEAT AND ORDERLY MANNER THROUGHOUT THE CONSTRUCTION PROCESS. STORE ALL MATERIALS WITHIN APPROVED STAGING AREAS.
12. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO BE FULLY INFORMED OF AND TO COMPLY WITH ALL PERMIT CONDITIONS, LAWS, ORDINANCES, CODES, REQUIREMENTS AND STANDARDS, WHICH IN ANY MANNER AFFECT THE COURSE OF CONSTRUCTION OF THIS PROJECT, THOSE ENGAGED OR EMPLOYED IN THE CONSTRUCTION AND THE MATERIALS USED IN THE CONSTRUCTION.
13. PROVIDE, AT CONTRACTOR'S SOLE EXPENSE, ALL MATERIALS, LABOR AND EQUIPMENT REQUIRED TO COMPLY WITH ALL APPLICABLE PERMIT CONDITIONS AND REQUIREMENTS.
14. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION STAKING AND LAYOUT, UNLESS OTHERWISE SPECIFIED.
15. FIELD INSPECTIONS AND OR THE PROVISION OF CONSTRUCTION STAKES DO NOT RELIEVE THE CONTRACTOR OF THEIR SOLE RESPONSIBILITY FOR ESTABLISHING ACCURATE CONSTRUCTED LINES AND GRADES, AS SPECIFIED.
16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND PRESERVATION OF ALL SURVEY MONUMENTS OR PROPERTY CORNERS. DISTURBED MONUMENTS SHALL BE RESTORED BACK TO THEIR ORIGINAL LOCATION AND SHALL BE CERTIFIED BY A REGISTERED CIVIL ENGINEER OR LAND SURVEYOR AT THE SOLE EXPENSE OF THE CONTRACTOR.
17. THE OWNER SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION OF ALL PROPERTY LINES AND EASEMENTS AND CONFIRMING THAT PROPOSED PROJECT ELEMENTS ARE LOCATED ON DISTRICT OWNED LANDS OR ARE COORDINATED WITH OWNERS AND APPROPRIATE PERMISSIONS ARE GRANTED FOR THE WORK.
18. TREE DIMENSIONS: TRUNK DIAMETERS SHOWN REPRESENT DIAMETER AT BREAST HEIGHT (DBH), MEASURED IN INCHES. DBH IS MEASURED 4.5 FT ABOVE GROUND FOR SINGLE TRUNKS AND TRUNKS THAT SPLIT INTO SEVERAL STEMS CLOSE TO THE GROUND. THE DBH FOR TREES THAT SPLIT INTO SEVERAL STEMS CLOSE TO THE GROUND MAY BE CONSOLIDATED INTO A SINGLE DBH BY TAKING THE SQUARE ROOT OF THE SUM OF ALL SQUARED STEM DBH'S, UNLESS OTHERWISE NOTED. WHERE TREES FORK NEAR BREAST HEIGHT, TRUNK DIAMETER IS MEASURED AT THE NARROWEST PART OF THE MAIN STEM BELOW THE FORK. FOR TREES ON A SLOPE, BREAST HEIGHT IS REFERENCED FROM THE UPPER SIDE OF THE SLOPE. FOR LEANING TREES, BREAST HEIGHT IS MEASURED ON THE SIDE THAT THE TREE LEANS TOWARD. TREES WITH DBH LESS THAN 8" ARE TYPICALLY NOT SHOWN.

12" P = 12" DBH PINE
19. TREE SPECIES ARE IDENTIFIED WHEN KNOWN. HOWEVER, FINAL DETERMINATION SHOULD BE MADE BY A QUALIFIED BOTANIST. REFER TO THE LEGEND FOR TREE SPECIES SYMBOLS.
20. TREE TRUNK DIMENSIONS MAY BE SHOWN OUT-OF-SCALE FOR PLOTTING CLARITY. CAUTION SHOULD BE USED IN DESIGNING NEAR TREE TRUNKS. THERE ARE LIMITATIONS ON FIELD ACCURACY, DRAFTING ACCURACY, MEDIUM STRETCH AS WELL AS THE "SPREAD" OR "LEANING" OF TREES. REQUEST ADDITIONAL TOPOGRAPHIC DETAIL WHERE CLOSE TOLERANCES ARE ANTICIPATED. INDIVIDUAL TREES ARE NOT TYPICALLY LOCATED WITHIN DRIFLINE CANOPY AREAS SHOWN.
21. APPROXIMATE CENSUS OF TREES TO BE REMOVED:

COMMON NAME	NUMBER
PINE	6
29. CONTRACTOR IS REQUIRED TO ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THE PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY; THIS REQUIREMENT SHALL BE MADE TO APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.
30. THE CONTRACTOR SHALL CONFORM TO THE RULES AND REGULATIONS OF THE CONSTRUCTION SAFETY ORDERS OF THE CALIFORNIA DIVISION OF OCCUPATIONAL SAFETY AND HEALTH PERTAINING TO EXCAVATION AND TRENCHES THE CALIFORNIA CODE OF REGULATIONS TITLE 8, SUBCHAPTER 4 CONSTRUCTION SAFETY ORDERS, ARTICLE 6 EXCAVATION.
31. CULTURAL RESOURCES: IN THE EVENT THAT HUMAN REMAINS AND/OR CULTURAL MATERIALS ARE FOUND, ALL PROJECT-RELATED CONSTRUCTION SHALL CEASE WITHIN A 100-FOOT RADIUS. THE CONTRACTOR SHALL NOTIFY THE YAKAMA NATION IMMEDIATELY.

EARTHWORK NOTES

1. GEOTECHNICAL SAMPLING AND ANALYSIS WILL BE PERFORMED PRIOR TO CONSTRUCTION AND A REPORT WITH EARTHWORK REQUIREMENTS WILL BE PROVIDED TO THE CONTRACTOR. NO WORK SHALL BE PERFORMED UNTIL THIS REPORT HAS BEEN ISSUED TO THE CONTRACTOR BY THE OWNER'S REPRESENTATIVE..
2. GRADING SUMMARY:

TOTAL CUT VOLUME =	906.3 CY
TOTAL FILL VOLUME =	32.0 CY
NET (CUT/FILL) =	874.3 CY

THE ABOVE QUANTITIES ARE APPROXIMATE IN-PLACE VOLUMES CALCULATED AS THE DIFFERENCE BETWEEN EXISTING GROUND AND THE PROPOSED FINISH GRADE, PREPARED FOR PERMITTING PURPOSES ONLY. EXISTING GROUND IS DEFINED BY THE TOPOGRAPHIC CONTOURS AND/OR SPOT ELEVATIONS ON THE PLAN. PROPOSED FINISH GRADE IS DEFINED AS THE DESIGN SURFACE ELEVATION OF WORK TO BE CONSTRUCTED. THE QUANTITIES HAVE NOT BEEN FACTORED TO INCLUDE ALLOWANCES FOR BULKING, CLEARING AND GRUBBING, SUBSIDENCE, SHRINKAGE, OVER EXCAVATION, AND RECOMPACTION, UNDERGROUND UTILITY AND SUBSTRUCTURE SPOILS AND CONSTRUCTION METHODS.

THE CONTRACTOR SHALL PERFORM AN INDEPENDENT EARTHWORK ESTIMATE FOR THE PURPOSE OF PREPARING BID PRICES FOR EARTHWORK. THE BID PRICE SHALL INCLUDE COSTS FOR ANY NECESSARY IMPORT AND PLACEMENT OF EARTH MATERIALS OR THE EXPORT AND PROPER DISPOSAL OF EXCESS OR UNSUITABLE EARTH MATERIALS.
3. PRIOR TO COMMENCING WORK, PROTECT ALL SENSITIVE AREAS TO REMAIN UNDISTURBED WITH TEMPORARY FENCING, AS SHOWN ON THE DRAWINGS, AS SPECIFIED, OR AS DIRECTED BY THE ENGINEER.
4. DO NOT DISTURB AREAS OUTSIDE OF THE DESIGNATED LIMITS OF DISTURBANCE, UNLESS AUTHORIZED IN WRITING BY THE ENGINEER. THE COST OF ALL ADDITIONAL WORK ASSOCIATED WITH RESTORATION AND REVEGETATION OF DISTURBED AREAS OUTSIDE THE DESIGNATED LIMITS OF DISTURBANCE, AS SHOWN ON THE DRAWINGS, SHALL BE BORNE SOLELY BY THE CONTRACTOR.
5. REMOVE ALL EXCESS SOILS TO AN APPROVED DUMP SITE OR DISPOSE OF ON SITE AT A LOCATION TO BE APPROVED BY THE ENGINEER, IN A MANNER THAT WILL NOT CAUSE EROSION.
6. CLEARING AND GRUBBING, SUBGRADE PREPARATION AND EARTHWORK SHALL BE PERFORMED IN ACCORDANCE WITH DIVISION 2 OF THE STANDARD SPECIFICATIONS, THESE DRAWINGS, AND THE TECHNICAL SPECIFICATIONS.
7. PRIOR TO STARTING WORK ON THE PROJECT, SUBMIT FOR ACCEPTANCE BY THE ENGINEER A HAZARDOUS MATERIALS CONTROLS AND SPILL PREVENTION PLAN. INCLUDE PROVISIONS FOR PREVENTING HAZARDOUS MATERIALS FROM CONTAMINATING SOIL OR ENTERING WATER COURSES, AND ESTABLISH A SPILL PREVENTION AND COUNTERMEASURE PLAN.
9. UNLESS AUTHORIZED BY THE GEOTECHNICAL ENGINEER, THE FOLLOWING MATERIALS SHALL NOT BE INCORPORATED INTO THE WORK:
 - A. ORGANIC MATERIALS SUCH AS PEAT, MULCH, ORGANIC SILT OR SOD.
 - B. SOILS CONTAINING EXPANSIVE CLAYS.
 - C. MATERIAL CONTAINING EXCESSIVE MOISTURE.
 - D. POORLY GRADED COURSE MATERIAL
 - E. PARTICLE SIZES IN EXCESS OF 6 INCHES.
 - E. MATERIAL WHICH WILL NOT ACHIEVE SPECIFIED DENSITY OR BEARING.
10. FINE GRADING ELEVATIONS, CONFORMS, AND SLOPES NOT CLEARLY SHOWN ON THE DRAWINGS SHALL BE DETERMINED BY THE CONTRACTOR IN THE FIELD TO DIRECT DRAINAGE TO PROTECTED DRAINAGE CONTROL STRUCTURES OR NATURAL WATERWAYS IN A MANNER THAT SUPPORTS THE INTENT OF THE DESIGN. ALL FINAL GRADING SHALL BE SUBJECT TO APPROVAL OF THE ENGINEER.
11. THE TOP 6" OF SUBGRADE UNDER ALL PAVED SURFACES SUBJECT TO VEHICULAR USE SHALL BE COMPACTED TO A MINIMUM OF 95% RELATIVE COMPACTION, IN ACCORDANCE WITH ASTM-D1557. ALL OTHER FILL TO BE COMPACTED TO A MINIMUM OF 90% MAXIMUM DENSITY AS DETERMINED BY ASTM-D1557 AND SO CERTIFIED BY TESTS AND REPORTS FROM THE CIVIL ENGINEER IN CHARGE OF THE GRADING CERTIFICATION.
12. SPREAD FILL MATERIAL IN LIFTS OF APPROXIMATELY 8 INCHES, MOISTENED OR DRIED TO NEAR OPTIMUM MOISTURE CONTENT AND RECOMPACTED. THE MATERIALS FOR ENGINEERED FILL SHALL BE APPROVED BY A REGISTERED CIVIL ENGINEER. ANY IMPORTED MATERIALS MUST BE APPROVED BEFORE BEING BROUGHT TO THE SITE. THE MATERIALS USED SHALL BE FREE OF ORGANIC MATTER AND OTHER DELETERIOUS MATERIALS.
13. ALL CONTACT SURFACES BETWEEN ORIGINAL GROUND AND RECOMPACTED FILL SHALL BE EITHER HORIZONTAL OR VERTICAL. ALL ORGANIC MATERIAL SHALL BE REMOVED AND THE REMAINING SURFACE SCARIFIED TO A DEPTH OF AT LEAST 12 INCHES, UNLESS DEEPER EXCAVATION IS REQUIRED BY THE ENGINEER.

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JAKE D. HOFELD

PREPARED AT THE REQUEST OF:
YAKAMA NATION FISHERIES

NOTES

BRUSH CREEK FISH PASSAGE AT 175 ROAD 100% DESIGN

DESIGNED BY: J.H.
 DRAWN BY: D.H./M.M.
 CHECKED BY: J.H.
 DATE: 9/11/2025
 JOB NO.: 20-026

BAR IS ONE INCH ON ORIGINAL DRAWING, ADJUST SCALES FOR REDUCED PLOTS
 0 1"

REV.	DATE	DESCRIPTION	BY
△	12/4/24	NO CHANGES THIS SHEET	M.M.
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HIP 4 GENERAL CONSERVATION MEASURES APPLICABLE TO ALL ACTIONS

THE ACTIVITIES COVERED UNDER THE HIP ARE INTENDED TO PROTECT AND RESTORE FISH AND WILDLIFE HABITAT WITH LONG-TERM BENEFITS TO ESA-LISTED SPECIES. THE FOLLOWING GENERAL CONSERVATION MEASURES (DEVELOPED IN COORDINATION WITH USFWS AND NMFS) WILL BE APPLIED TO ALL ACTIONS OF THIS PROJECT.

PROJECT DESIGN AND SITE PREPARATION

1. STATE AND FEDERAL PERMITS

- 1.A. ALL APPLICABLE REGULATORY PERMITS AND OFFICIAL PROJECT AUTHORIZATIONS WILL BE OBTAINED BEFORE PROJECT IMPLEMENTATION.
- 1.B. THESE PERMITS AND AUTHORIZATIONS INCLUDE, BUT ARE NOT LIMITED TO, NATIONAL ENVIRONMENTAL POLICY ACT, NATIONAL HISTORIC PRESERVATION ACT, THE APPROPRIATE STATE AGENCY REMOVAL AND FILL PERMIT, USACE CLEAN WATER ACT (CWA) 404 PERMITS, CWA SECTION 401 WATER QUALITY CERTIFICATIONS, AND FEMA NO-RISE ANALYSES.

2. TIMING OF IN-WATER WORK

- 2.A. APPROPRIATE STATE (OREGON DEPARTMENT OF FISH AND WILDLIFE (ODFW), WASHINGTON DEPARTMENT OF FISH AND WILDLIFE (WDFW), IDAHO DEPARTMENT OF FISH AND GAME (IDFG), AND MONTANA FISH WILDLIFE AND PARKS (MFWP)) GUIDELINES FOR TIMING OF IN-WATER WORK WINDOWS (IWW) WILL BE FOLLOWED.
- 2.B. CHANGES TO ESTABLISHED WORK WINDOWS WILL BE APPROVED BY REGIONAL STATE BIOLOGISTS AND BPA'S EC LEAD.
- 2.C. BULL TROUT. FOR AREAS WITH DESIGNATED IN-WATER WORK WINDOWS FOR BULL TROUT OR AREAS KNOWN TO HAVE BULL TROUT, PROJECT PROPONENTS WILL CONTACT THE APPROPRIATE USFWS FIELD OFFICE TO INSURE THAT ALL REASONABLE IMPLEMENTATION MEASURES ARE CONSIDERED AND AN APPROPRIATE IN-WATER WORK WINDOW IS BEING USED TO MINIMIZE PROJECT EFFECTS.
- 2.D. LAMPREY. WORKING IN STREAM OR RIVER CHANNELS THAT CONTAIN PACIFIC LAMPREY WILL BE AVOIDED FROM MARCH 1 TO JULY 1 FOR REACHES <5,000 FEET IN ELEVATION AND FROM MARCH 1 TO AUGUST 1 FOR REACHES >5,000 FEET. IF EITHER TIMEFRAME IS INCOMPATIBLE WITH OTHER OBJECTIVES, THE AREA WILL BE SURVEYED FOR NESTS AND LAMPREY PRESENCE, AND AVOIDED IF POSSIBLE. IF LAMPREYS ARE KNOWN TO EXIST, THE PROJECT SPONSOR WILL UTILIZE DEWATERING AND SALVAGE PROCEDURES (SEE FISH SALVAGE AND ELECTROFISHING SECTIONS) TO MINIMIZE ADVERSE EFFECTS.
- 2.E. THE IN-WATER WORK WINDOW WILL BE PROVIDED IN THE CONSTRUCTION PLANS.

3. CONTAMINANTS

- 3.A. EXCAVATION OF MORE THAN 20 CUBIC YARDS WILL REQUIRE A SITE VISIT AND DOCUMENTED ASSESSMENT FOR POTENTIAL CONTAMINANT SOURCES. THE SITE ASSESSMENT WILL BE STORED WITH PROJECT FILES OR AS AN APPENDIX TO THE BASIS OF DESIGN REPORT.
- 3.B. THE SITE ASSESSMENT WILL SUMMARIZE:
 - 3.B.1. THE SITE VISIT, CONDITION OF THE PROPERTY, AND IDENTIFICATION OF ANY AREAS USED FOR VARIOUS INDUSTRIAL PROCESSES;
 - 3.B.2. AVAILABLE RECORDS, SUCH AS FORMER SITE USE, BUILDING PLANS, AND RECORDS OF ANY PRIOR CONTAMINATION EVENTS;
 - 3.B.3. INTERVIEWS WITH KNOWLEDGEABLE PEOPLE, SUCH AS SITE OWNERS, OPERATORS, OCCUPANTS, NEIGHBORS, OR LOCAL GOVERNMENT OFFICIALS; AND
 - 3.B.4. THE TYPE, QUANTITY, AND EXTENT OF ANY POTENTIAL CONTAMINATION SOURCES.

4. SITE LAYOUT AND FLAGGING

- 4.A. CONSTRUCTION AREAS TO BE CLEARLY FLAGGED PRIOR TO CONSTRUCTION.
- 4.B. AREAS TO BE FLAGGED WILL INCLUDE:
 - 4.B.1. SENSITIVE RESOURCE AREAS, SUCH AS AREAS BELOW ORDINARY HIGH WATER, SPAWNING AREAS, SPRINGS, AND WETLANDS;
 - 4.B.2. EQUIPMENT ENTRY AND EXIT POINTS;
 - 4.B.3. ROAD AND STREAM CROSSING ALIGNMENTS;
 - 4.B.4. STAGING, STORAGE, AND STOCKPILE AREAS; AND
 - 4.B.5. NO-SPRAY AREAS AND BUFFERS.

5. TEMPORARY ACCESS ROADS AND PATHS

- 5.A. EXISTING ACCESS ROADS AND PATHS WILL BE PREFERENTIALLY USED WHENEVER REASONABLE, AND THE NUMBER AND LENGTH OF TEMPORARY ACCESS ROADS AND PATHS THROUGH RIPARIAN AREAS AND FLOODPLAINS WILL BE MINIMIZED.
- 5.B. VEHICLE USE AND HUMAN ACTIVITIES, INCLUDING WALKING, IN AREAS OCCUPIED BY TERRESTRIAL ESA-LISTED SPECIES WILL BE MINIMIZED.
- 5.C. TEMPORARY ACCESS ROADS AND PATHS WILL NOT BE BUILT ON SLOPES WHERE GRADE, SOIL, OR OTHER FEATURES SUGGEST A LIKELIHOOD OF EXCESSIVE EROSION OR FAILURE. IF SLOPES ARE STEEPER THAN 30%, THEN THE ROAD WILL BE DESIGNED BY A CIVIL ENGINEER WITH EXPERIENCE IN STEEP ROAD DESIGN.
- 5.D. THE REMOVAL OF RIPARIAN VEGETATION DURING CONSTRUCTION OF TEMPORARY ACCESS ROADS WILL BE MINIMIZED. WHEN TEMPORARY VEGETATION REMOVAL IS REQUIRED, VEGETATION WILL BE CUT AT GROUND LEVEL (NOT GRUBBED).
- 5.E. AT PROJECT COMPLETION, ALL TEMPORARY ACCESS ROADS AND PATHS WILL BE OBLITERATED, AND THE SOIL WILL BE STABILIZED AND REVEGETATED. ROAD AND PATH OBLITERATION REFERS TO THE MOST COMPREHENSIVE DEGREE OF DECOMMISSIONING AND INVOLVES DECOMPACTING THE SURFACE AND DITCH, PULLING THE FILL MATERIAL ONTO THE RUNNING SURFACE, AND RESHAPING TO MATCH THE ORIGINAL CONTOUR.
- 5.F. HELICOPTER FLIGHT PATTERNS WILL BE ESTABLISHED IN ADVANCE AND LOCATED TO AVOID TERRESTRIAL ESA-LISTED SPECIES AND THEIR OCCUPIED HABITAT DURING SENSITIVE LIFE STAGES.

6. TEMPORARY STREAM CROSSINGS

- 6.A. EXISTING STREAM CROSSINGS OR BEDROCK WILL BE PREFERENTIALLY USED WHENEVER REASONABLE, AND THE NUMBER OF TEMPORARY STREAM CROSSINGS WILL BE MINIMIZED.
- 6.B. TEMPORARY BRIDGES AND CULVERTS WILL BE INSTALLED TO ALLOW FOR EQUIPMENT AND VEHICLE CROSSING OVER PERENNIAL STREAMS DURING CONSTRUCTION. TREATED WOOD SHALL NOT BE USED ON TEMPORARY BRIDGE CROSSINGS OR IN LOCATIONS IN CONTACT WITH OR DIRECTLY OVER WATER.
- 6.C. FOR PROJECTS THAT REQUIRE EQUIPMENT AND VEHICLES TO CROSS IN THE WET:
 - 6.C.1. THE LOCATION AND NUMBER OF ALL WET CROSSINGS SHALL BE APPROVED BY THE BPA EC LEAD AND DOCUMENTED IN THE CONSTRUCTION PLANS;
 - 6.C.2. VEHICLES AND MACHINERY SHALL CROSS STREAMS AT RIGHT ANGLES TO THE MAIN CHANNEL WHENEVER POSSIBLE;
 - 6.C.3. NO STREAM CROSSINGS WILL OCCUR 300 FEET UPSTREAM OR 100 FEET DOWNSTREAM OF AN EXISTING REDD OR SPAWNING FISH; AND
 - 6.C.4. AFTER PROJECT COMPLETION, TEMPORARY STREAM CROSSINGS WILL BE OBLITERATED AND BANKS RESTORED.

7. STAGING, STORAGE, AND STOCKPILE AREAS

- 7.A. STAGING AREAS (USED FOR CONSTRUCTION EQUIPMENT STORAGE, VEHICLE STORAGE, FUELING, SERVICING, AND HAZARDOUS MATERIAL STORAGE) WILL BE 150 FEET OR MORE FROM ANY NATURAL WATER BODY OR WETLAND. STAGING AREAS CLOSER THAN 150 FEET WILL BE APPROVED BY THE EC LEAD.
- 7.B. NATURAL MATERIALS USED FOR IMPLEMENTATION OF AQUATIC RESTORATION, SUCH AS LARGE WOOD, GRAVEL, AND BOULDERS, MAY BE STAGED WITHIN 150 FEET IF CLEARLY INDICATED IN THE PLANS THAT AREA IS FOR NATURAL MATERIALS ONLY.
- 7.C. ANY LARGE WOOD, TOPSOIL, AND NATIVE CHANNEL MATERIAL DISPLACED BY CONSTRUCTION WILL BE STOCKPILED FOR USE DURING SITE RESTORATION AT A SPECIFICALLY IDENTIFIED AND FLAGGED AREA.
- 7.D. ANY MATERIAL NOT USED IN RESTORATION, AND NOT NATIVE TO THE FLOODPLAIN, WILL BE DISPOSED OF OUTSIDE THE 100-YEAR FLOODPLAIN.

8. EQUIPMENT

- 8.A. MECHANIZED EQUIPMENT AND VEHICLES WILL BE SELECTED, OPERATED, AND MAINTAINED IN A MANNER THAT MINIMIZES ADVERSE EFFECTS ON THE ENVIRONMENT (E.G., MINIMALLY-SIZED, LOW PRESSURE TIRES; MINIMAL HARD-TURN PATHS FOR TRACKED VEHICLES; TEMPORARY MATS OR PLATES WITHIN WET AREAS OR ON SENSITIVE SOILS).
- 8.B. EQUIPMENT WILL BE STORED, FUELED, AND MAINTAINED IN AN CLEARLY IDENTIFIED STAGING AREA THAT MEETS STAGING AREA CONSERVATION MEASURES.
- 8.C. EQUIPMENT WILL BE REFUELED IN A VEHICLE STAGING AREA OR IN AN ISOLATED HARD ZONE, SUCH AS A PAVED PARKING LOT OR ADJACENT, ESTABLISHED ROAD (THIS MEASURE APPLIES ONLY TO GAS-POWERED EQUIPMENT WITH TANKS LARGER THAN 5 GALLONS).
- 8.D. BIODEGRADABLE LUBRICANTS AND FLUIDS WILL BE USED ON EQUIPMENT OPERATING IN AND ADJACENT TO THE STREAM CHANNEL AND LIVE WATER.
- 8.E. EQUIPMENT WILL BE INSPECTED DAILY FOR FLUID LEAKS BEFORE LEAVING THE VEHICLE STAGING AREA FOR OPERATION WITHIN 150 FEET OF ANY NATURAL WATER BODY OR WETLAND.
- 8.F. EQUIPMENT WILL BE THOROUGHLY CLEANED BEFORE OPERATION BELOW ORDINARY HIGH WATER, AND AS OFTEN AS NECESSARY DURING OPERATION, TO REMAIN GREASE FREE.

9. EROSION CONTROL

- 9.A. TEMPORARY EROSION CONTROL MEASURES INCLUDE:
 - 9.A.1. TEMPORARY EROSION CONTROLS WILL BE IN PLACE BEFORE ANY SIGNIFICANT ALTERATION OF THE ACTION SITE AND APPROPRIATELY INSTALLED DOWNSLOPE OF PROJECT ACTIVITY WITHIN THE RIPARIAN BUFFER AREA UNTIL SITE REHABILITATION IS COMPLETE;
 - 9.A.2. IF THERE IS A POTENTIAL FOR ERODED SEDIMENT TO ENTER THE STREAM, SEDIMENT BARRIERS WILL BE INSTALLED AND MAINTAINED FOR THE DURATION OF PROJECT IMPLEMENTATION;
 - 9.A.3. TEMPORARY EROSION CONTROL MEASURES MAY INCLUDE SEDGE MATS, FIBER WATTLES, SILT FENCES, JUTE MATTING, WOOD FIBER MULCH AND SOIL BINDER, OR GEOTEXTILES AND GEOSYNTHETIC FABRIC;
 - 9.A.4. SOIL STABILIZATION UTILIZING WOOD FIBER MULCH AND TACKIFIER (HYDRO-APPLIED) MAY BE USED TO REDUCE EROSION OF BARE SOIL IF THE MATERIALS ARE NOXIOUS WEED FREE AND NONTOXIC TO AQUATIC AND TERRESTRIAL ANIMALS, SOIL MICROORGANISMS, AND VEGETATION;
 - 9.A.5. SEDIMENT WILL BE REMOVED FROM EROSION CONTROLS ONCE IT HAS REACHED 1/3 OF THE EXPOSED HEIGHT OF THE CONTROL; AND
 - 9.A.6. ONCE THE SITE IS STABILIZED AFTER CONSTRUCTION, TEMPORARY EROSION CONTROL MEASURES WILL BE REMOVED.
- 9.B. EMERGENCY EROSION CONTROLS. THE FOLLOWING MATERIALS FOR EMERGENCY EROSION CONTROL WILL BE AVAILABLE AT THE WORK SITE:
 - 9.B.1. A SUPPLY OF SEDIMENT CONTROL MATERIALS; AND
 - 9.B.2. AN OIL-ABSORBING FLOATING BOOM WHENEVER SURFACE WATER IS PRESENT.

10. DUST ABATEMENT

- 10.A. THE PROJECT SPONSOR WILL DETERMINE THE APPROPRIATE DUST CONTROL MEASURES BY CONSIDERING SOIL TYPE, EQUIPMENT USAGE, PREVAILING WIND DIRECTION, AND THE EFFECTS CAUSED BY OTHER EROSION AND SEDIMENT CONTROL MEASURES.
- 10.B. WORK WILL BE SEQUENCED AND SCHEDULED TO REDUCE EXPOSED BARE SOIL SUBJECT TO WIND EROSION.
- 10.C. DUST-ABATEMENT ADDITIVES AND STABILIZATION CHEMICALS (TYPICALLY MAGNESIUM CHLORIDE, CALCIUM CHLORIDE SALTS, OR LIGNINSULFONATE) WILL NOT BE APPLIED WITHIN 25 FEET OF WATER OR A STREAM CHANNEL AND WILL BE APPLIED SO AS TO MINIMIZE THE LIKELIHOOD THAT THEY WILL ENTER STREAMS. APPLICATIONS OF LIGNINSULFONATE WILL BE LIMITED TO A MAXIMUM RATE OF 0.5 GALLONS PER SQUARE YARD OF ROAD SURFACE, ASSUMING MIXED 50:50 WITH WATER.
- 10.D. APPLICATION OF DUST ABATEMENT CHEMICALS WILL BE AVOIDED DURING OR JUST BEFORE WET WEATHER, AND AT STREAM CROSSINGS OR OTHER AREAS THAT COULD RESULT IN UNFILTERED DELIVERY OF THE DUST ABATEMENT MATERIALS TO A WATERBODY (TYPICALLY THESE WOULD BE AREAS WITHIN 25 FEET OF A WATERBODY OR STREAM CHANNEL; DISTANCES MAY BE GREATER WHERE VEGETATION IS SPARSE OR SLOPES ARE STEEP).
- 10.E. SPILL CONTAINMENT EQUIPMENT WILL BE AVAILABLE DURING APPLICATION OF DUST ABATEMENT CHEMICALS.
- 10.F. PETROLEUM-BASED PRODUCTS WILL NOT BE USED FOR DUST ABATEMENT.

11. SPILL PREVENTION, CONTROL, AND COUNTER MEASURES

- 11.A. A DESCRIPTION OF HAZARDOUS MATERIALS THAT WILL BE USED, INCLUDING INVENTORY, STORAGE, AND HANDLING PROCEDURES WILL BE AVAILABLE ON-SITE.
- 11.B. WRITTEN PROCEDURES FOR NOTIFYING ENVIRONMENTAL RESPONSE AGENCIES WILL BE POSTED AT THE WORK SITE.
- 11.C. SPILL CONTAINMENT KITS (INCLUDING INSTRUCTIONS FOR CLEANUP AND DISPOSAL) ADEQUATE FOR THE TYPES AND QUANTITY OF HAZARDOUS MATERIALS USED AT THE SITE WILL BE AVAILABLE AT THE WORK SITE.
- 11.D. WORKERS WILL BE TRAINED IN SPILL CONTAINMENT PROCEDURES AND WILL BE INFORMED OF THE LOCATION OF SPILL CONTAINMENT KITS.
- 11.E. ANY WASTE LIQUIDS GENERATED AT THE STAGING AREAS WILL BE TEMPORARILY STORED UNDER AN IMPERVIOUS COVER, SUCH AS A TARPULLIN, UNTIL THEY CAN BE PROPERLY TRANSPORTED TO AND DISPOSED OF AT A FACILITY THAT IS APPROVED FOR RECEIPT OF HAZARDOUS MATERIALS.
- 11.F. PUMPS USED ADJACENT TO WATER SHALL USE SPILL CONTAINMENT SYSTEMS.

12. INVASIVE SPECIES CONTROL

- 12.A. PRIOR TO ENTERING THE SITE, ALL VEHICLES AND EQUIPMENT WILL BE POWER WASHED, ALLOWED TO FULLY DRY, AND INSPECTED TO MAKE SURE NO PLANTS, SOIL, OR OTHER ORGANIC MATERIAL ADHERES TO THE SURFACE.
- 12.B. WATERCRAFT, WADERS, BOOTS, AND ANY OTHER GEAR TO BE USED IN OR NEAR WATER WILL BE INSPECTED FOR AQUATIC INVASIVE SPECIES.
- 12.C. WADING BOOTS WITH FELT SOLES ARE NOT TO BE USED DUE TO THEIR PROPENSITY FOR AIDING IN THE TRANSFER OF INVASIVE SPECIES UNLESS DECONTAMINATION PROCEDURES HAVE BEEN APPROVED BY THE EC LEAD.

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PREPARED AT THE REQUEST OF:
YAKAMA NATION FISHERIES

HIP 4 GENERAL CONSERVATION MEASURES (1 OF 2)

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